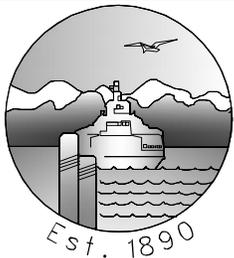


PVC: POINT OF VERTICAL CURVE
 PVI: POINT OF VERTICAL INTERSECTION
 PVT: POINT OF VERTICAL TANGENCY
 G1/G2: ROADWAY GRADES (%)



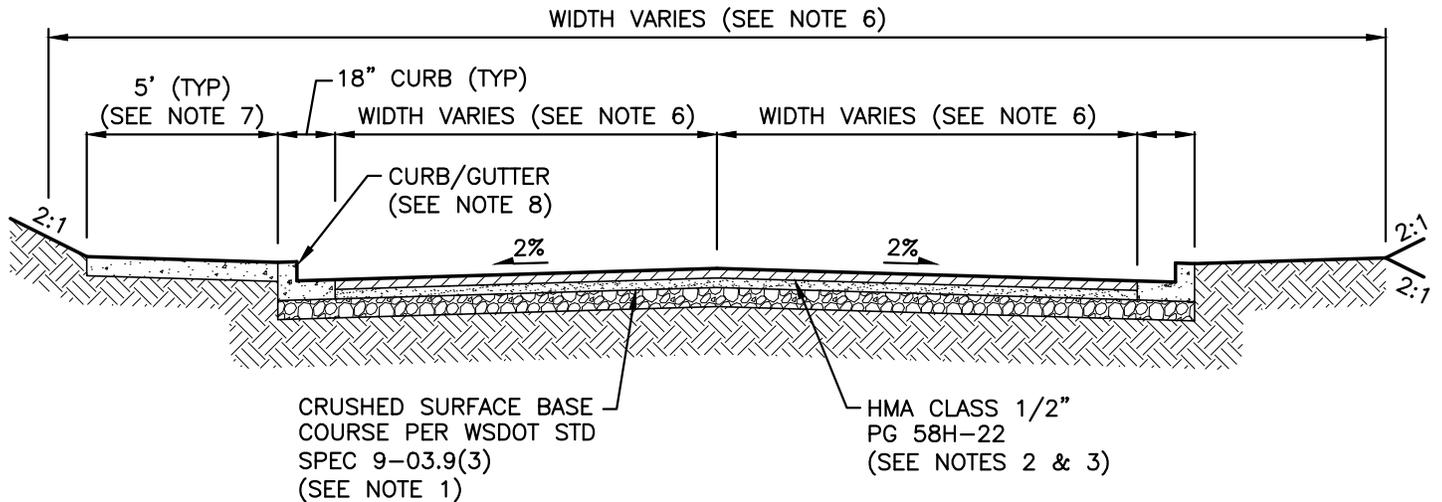
CITY OF EDMONDS
 PUBLIC WORKS
 DEPARTMENT

TYPICAL STREET PROFILE

APPROVED BY: **R. ENGLISH**

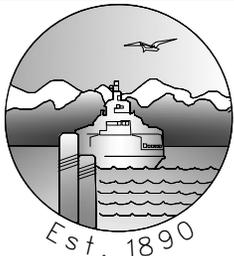
REVISION DATE
JANUARY 2018

STANDARD
 DETAIL
TR-500



NOTES:

1. CSBC DEPTH SHALL BE A MINIMUM 6" OR MATCH EXISTING WHICHEVER IS GREATER. CSBC SHALL BE INSTALLED IN MULTIPLE LIFTS OF EQUAL THICKNESS WHEN EXCEEDING 6" THICKNESS.
2. ROADWAY HMA DEPTH SHALL BE A MINIMUM OF 4" THICK. UNLESS APPROVED BY THE ENGINEER, ANY DEPTH GREATER THAN 4" SHALL MATCH EXISTING.
3. ALLEY HMA DEPTH SHALL BE A MINIMUM OF 2" THICK. UNLESS APPROVED BY THE ENGINEER, ANY DEPTH GREATER THAN 2" SHALL MATCH EXISTING.
4. UNLESS APPROVED BY THE ENGINEER, THE HMA SHALL BE INSTALLED IN MULTIPLE EQUAL THICKNESS LIFTS NOT EXCEEDING 2".
5. FINAL PAVEMENT JOINTS SHALL BE NEATLY AND UNIFORMLY SEALED PER WSDOT STANDARD SPECIFICATIONS 5-04.3(4)A.
6. REFER TO EDMONDS COMMUNITY DEVELOPMENT CODE TABLE OF STREET STANDARDS FOR RIGHT-OF-WAY AND PAVEMENT WIDTHS.
7. REFER TO EDMONDS COMMUNITY DEVELOPMENT CODE SIDEWALK STANDARDS FOR SIDEWALK REQUIREMENTS.
8. REFER TO CITY STANDARD CURB DETAILS.



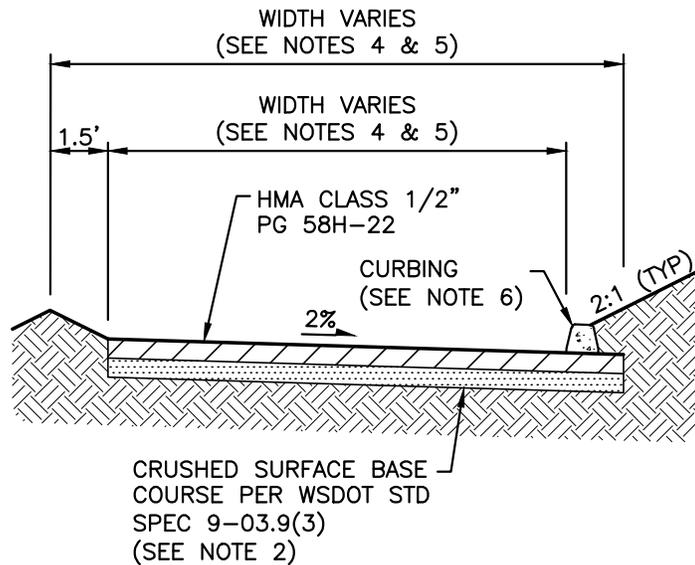
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

PUBLIC STREET

APPROVED BY: R. ENGLISH

REVISION DATE
FEBRUARY 2020

STANDARD
DETAIL
TR-505



NOTES:

1. CSBC DEPTH SHALL BE A MINIMUM 6" OR MATCH EXISTING WHICHEVER IS GREATER. CSBC SHALL BE INSTALLED IN MULTIPLE LIFTS OF EQUAL THICKNESS WHEN EXCEEDING 6" THICKNESS.
2. ROADWAY HMA DEPTH SHALL BE A MINIMUM OF 2" THICK. UNLESS APPROVED BY THE ENGINEER, ANY DEPTH GREATER THEN 2" SHALL MATCH EXISTING.
3. FINAL PAVEMENT JOINTS SHALL BE NEATLY AND UNIFORMLY SEALED PER WSDOT STANDARD SPECIFICATIONS 5-04.3(4)A.
4. REFER TO EDMONDS COMMUNITY DEVELOPMENT CODE TABLE OF STREET STANDARDS FOR EASEMENT AND PAVEMENT WIDTHS.
5. FIRE DEPARTMENT MAY REQUIRE ACCESS TO BE WIDER IF IT IS DETERMINED FIRE FIGHTING OR RESCUE OPERATIONS ARE HAMPERED.
6. 6" CONCRETE EXTRUDED CURB OR 18" ASPHALT THICKENED EDGE REQUIRED (REFER TO CITY STANDARD CURB DETAILS).



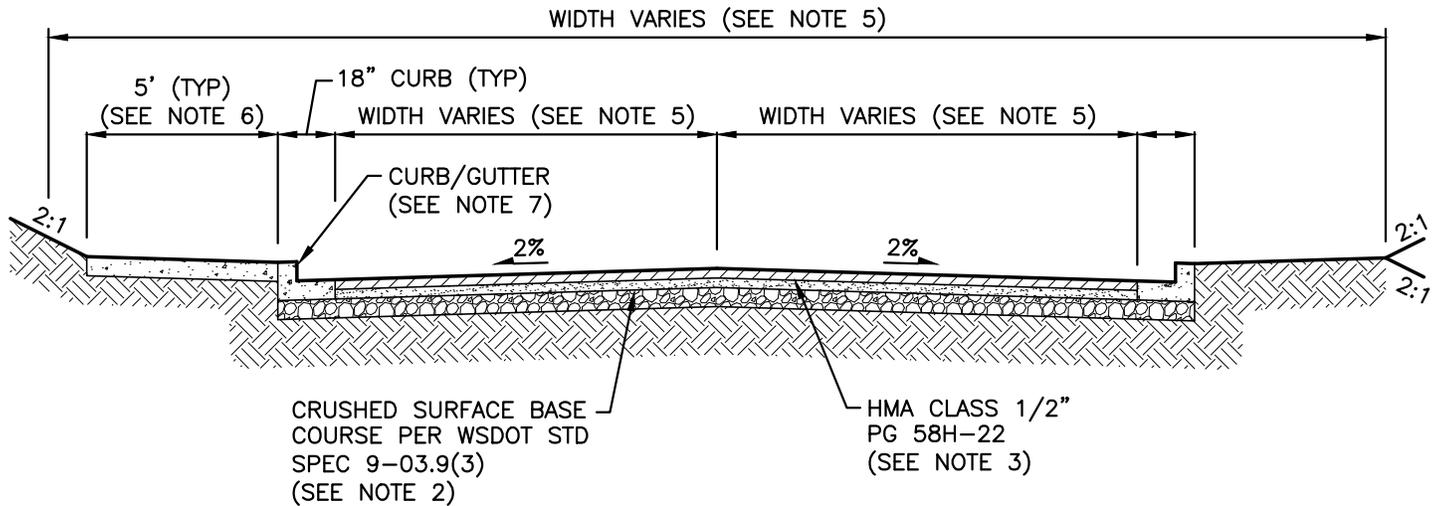
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**PRIVATE STREET
(1-4 LOTS)**

APPROVED BY: R. ENGLISH

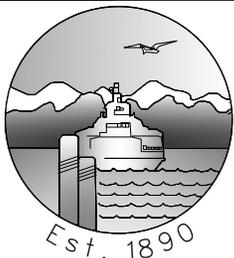
REVISION DATE
FEBRUARY 2020

STANDARD
DETAIL
TR-510



NOTES:

1. CSBC DEPTH SHALL BE A MINIMUM 6" OR MATCH EXISTING WHICHEVER IS GREATER. CSBC SHALL BE INSTALLED IN MULTIPLE LIFTS OF EQUAL THICKNESS WHEN EXCEEDING 6" THICKNESS.
2. ROADWAY HMA DEPTH SHALL BE A MINIMUM OF 4" THICK. UNLESS APPROVED BY THE ENGINEER, ANY DEPTH GREATER THAN 4" SHALL MATCH EXISTING.
3. UNLESS APPROVED BY THE ENGINEER, THE HMA SHALL BE INSTALLED IN MULTIPLE EQUAL THICKNESS LIFTS NOT EXCEEDING 2".
4. FINAL PAVEMENT JOINTS SHALL BE NEATLY AND UNIFORMLY SEALED PER WSDOT STANDARD SPECIFICATIONS 5-04.3(4)A.
5. REFER TO EDMONDS COMMUNITY DEVELOPMENT CODE TABLE OF STREET STANDARDS FOR RIGHT-OF-WAY AND PAVEMENT WIDTHS.
6. REFER TO EDMONDS COMMUNITY DEVELOPMENT CODE SIDEWALK STANDARDS FOR SIDEWALK REQUIREMENTS.
7. REFER TO CITY STANDARD CURB DETAILS.



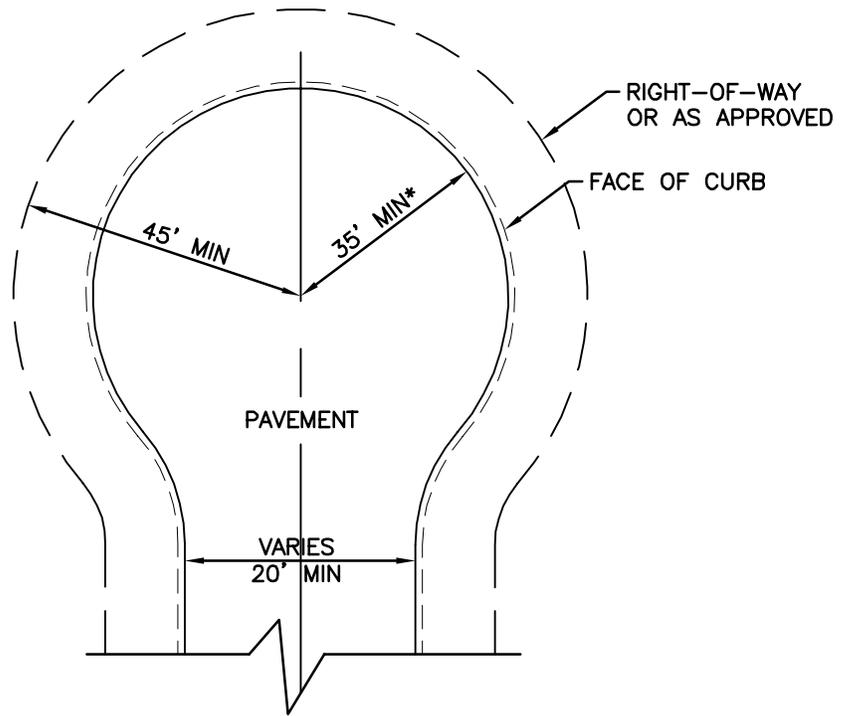
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**PRIVATE STREET
(5-9 LOTS)**

APPROVED BY: R. ENGLISH

REVISION DATE
FEBRUARY 2020

STANDARD
DETAIL
TR-511

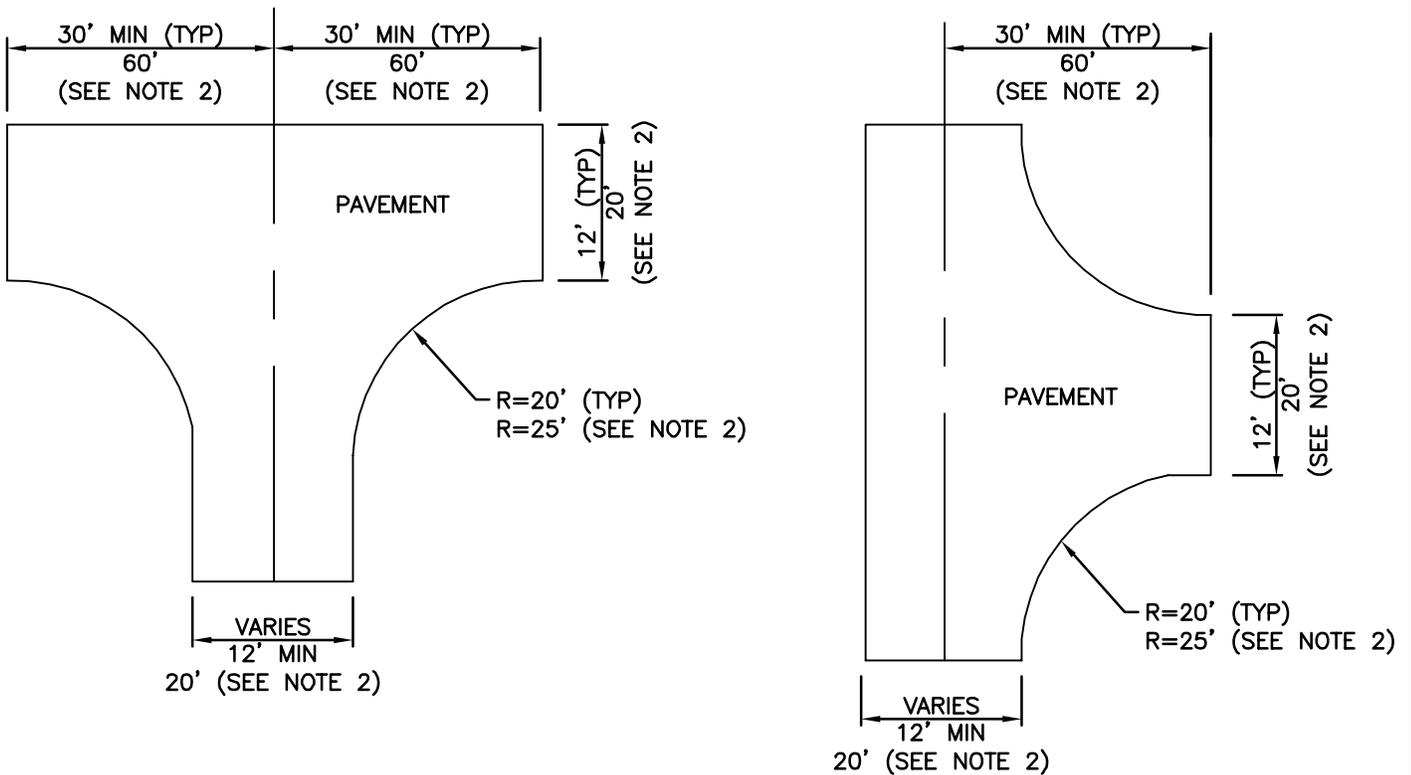


NOTES:

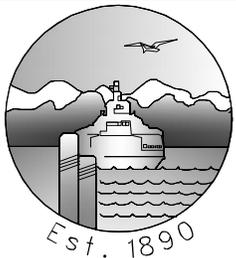
1. NO PARKING PERMITTED WITHIN TURN-AROUNDS OR CUL-DE-SACS IF PARKING WOULD REDUCE THE RADIUS OF CUL-DE-SAC TO LESS THAN 35'
2. TYPICAL WIDTH AND RADIUS AS REQUIRED TO COMPLY WITH SOUTH SNOHOMISH COUNTY REGIONAL FIRE (SSCRF)

TYPICAL CUL-DE-SAC

FOR PARKING, ADD 7.5' PER TRAVEL LANE
 * RADIUS OF ASPHALT AREA MAY VARY PER SSCRF



TYPICAL TURNAROUND



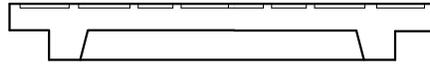
CITY OF EDMONDS
 PUBLIC WORKS
 DEPARTMENT

**CUL-DE-SAC
 AND TURN-AROUND
 DETAILS**

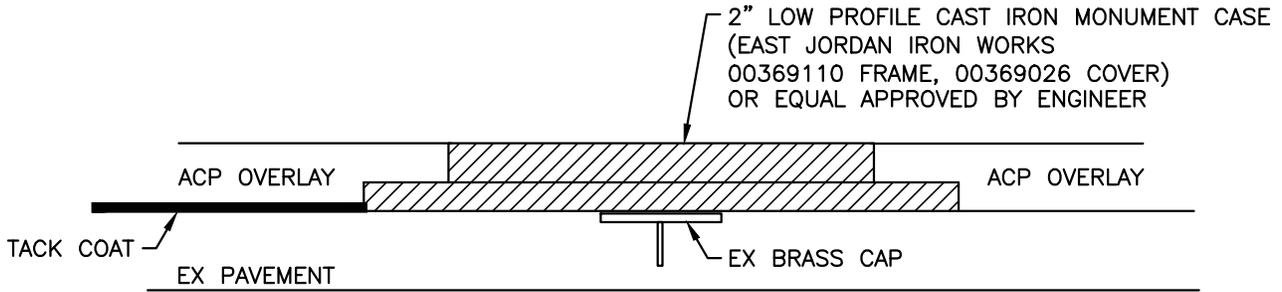
APPROVED BY: **R. ENGLISH**

REVISION DATE
MARCH 2018

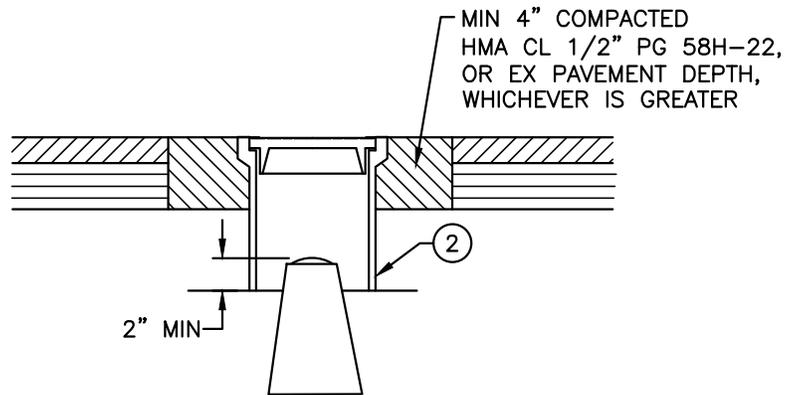
STANDARD
 DETAIL
TR-514



COVER



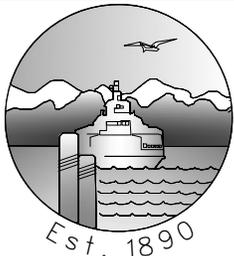
MONUMENT CASE (LOW PROFILE)



MONUMENT CASE (ADJUSTMENT)

NOTES:

- ① SURVEY MONUMENT CASES SHALL NOT BE ADJUSTED, BUT SHALL BE REMOVED AND REPLACED IF THE OVERLAY RESULTS IN A GRADE CHANGE. THE MONUMENT SHALL BE REFERENCED BY SURVEY PRIOR TO ANY CONSTRUCTION ACTIVITY IN ACCORDANCE WITH RCW 58.090.130.
- ② CONCRETE MONUMENT SHALL EXTEND A MINIMUM 2" INTO MONUMENT CASE. REFER TO DETAIL TR-516.



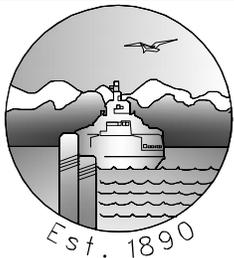
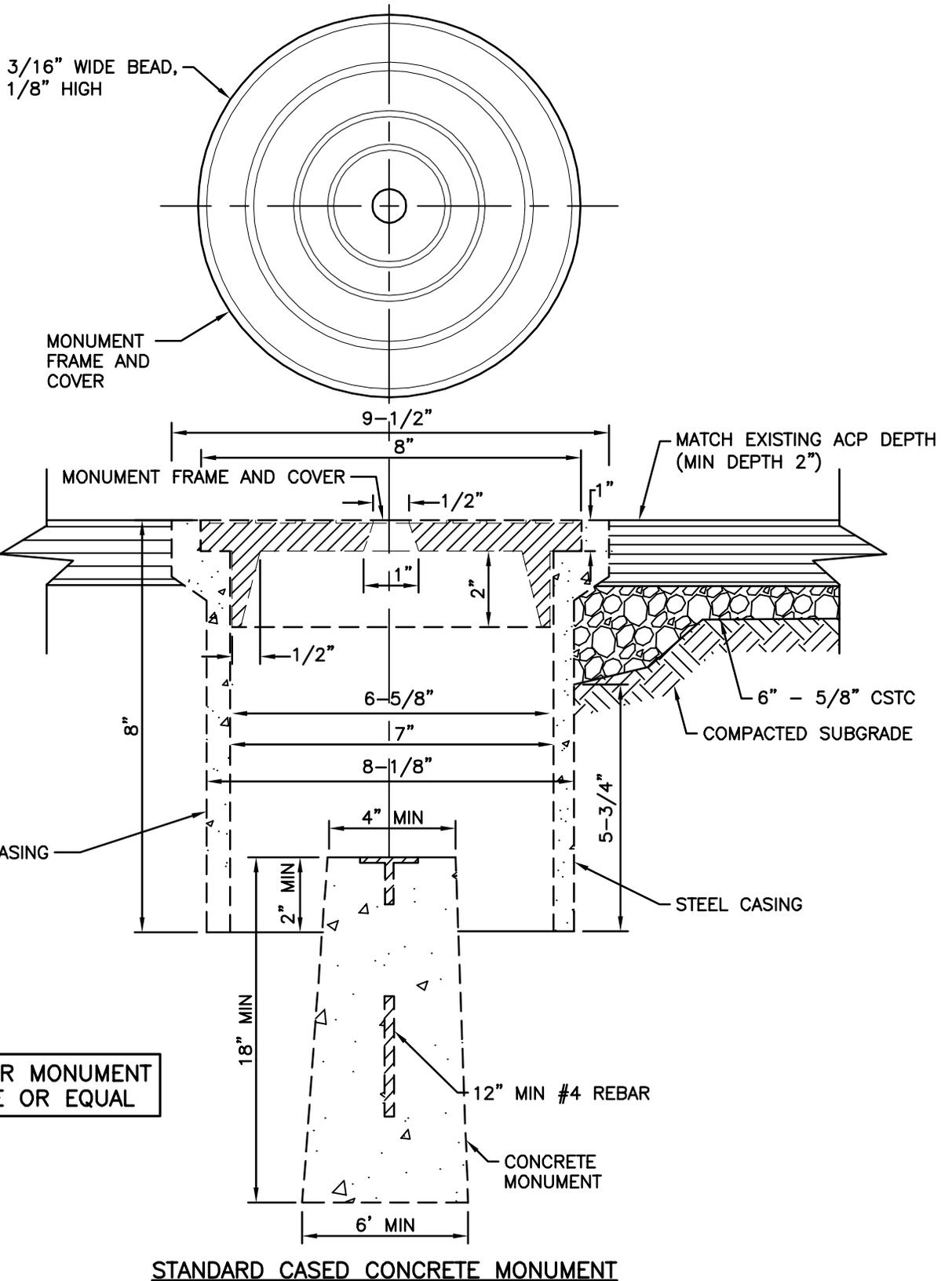
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**2" OVERLAY
ADJUSTMENT RISER —
BRASS CAP**

APPROVED BY: R. ENGLISH

REVISION DATE
FEBRUARY 2020

STANDARD
DETAIL
TR-515



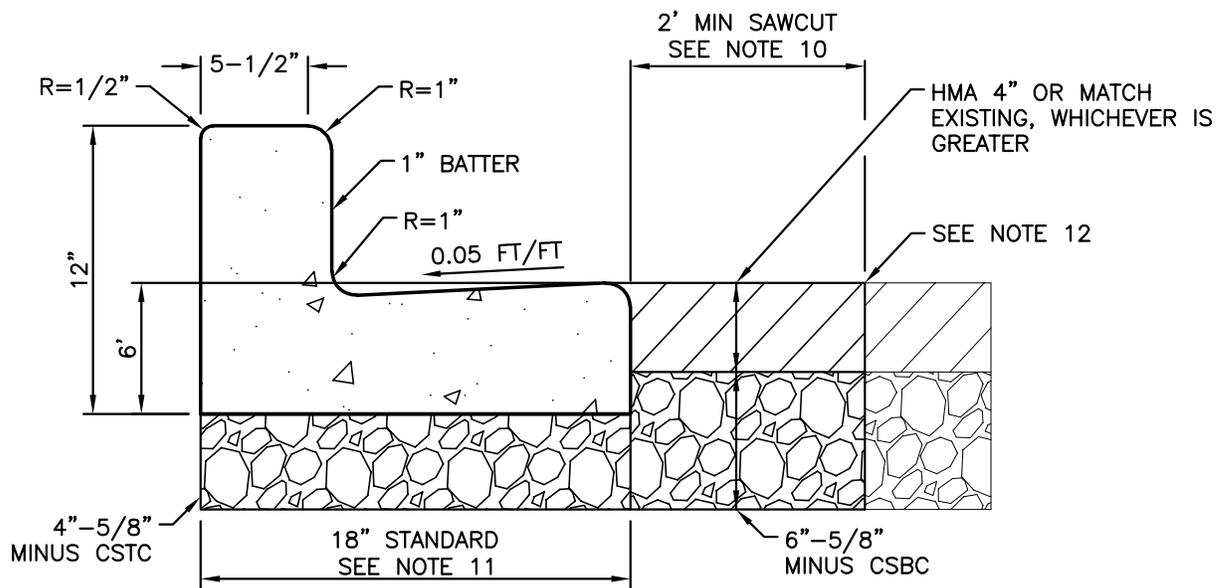
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**STREET MONUMENT
AND CASE**

REVISION DATE
JANUARY 2018

STANDARD
DETAIL
TR-516

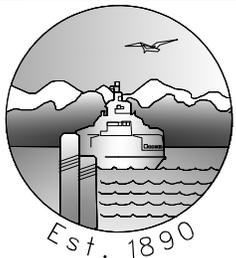
APPROVED BY: **R. ENGLISH**



STANDARD TYPE "A" CURB/GUTTER
(NOT TO SCALE)

NOTES:

1. CITY INSPECTION REQUIRED ON FORM WORK PRIOR TO POUR.
2. FORMS SHALL BE TRUE TO LINE AND GRADE AND SECURELY STAKED.
3. EXPANSION JOINTS SHALL BE PLACED ADJACENT TO ANY STRUCTURE.
4. EXPANSION JOINTS SHALL BE EVERY 10 FEET.
5. EXPANSION JOINTS SHALL HAVE 1/2" TO 5/8" WIDE PREMOLDED JOINT FILLER.
6. CONCRETE SHALL BE CLASS 3000.
7. FINISH SHALL BE LIGHT BROOM.
8. CURB SHALL BE SPRAYED WITH CLEAR CURING COMPOUND OR SHALL BE COVERED AND KEPT MOIST FOR 72 HOURS.
9. REMOVAL/REPLACEMENT OF CONCRETE CURB SHALL BE FROM EXPANSION TO EXPANSION JOINT, UNLESS OTHERWISE DIRECTED BY CITY ENGINEER.
10. A 2-FT MINIMUM ASPHALT SAWCUT MAY BE REQUIRED AS DETERMINED BY CITY ENGINEER.
11. CURB/GUTTER SHALL BE 18" OR AS DIRECTED BY CITY ENGINEER.
12. ALL VERTICAL EDGES SHALL BE TACKED.
13. NATIVE AND GRAVEL SUBGRADE SHALL BE COMPACTED TO A MINIMUM OF 95% MAX DENSITY.



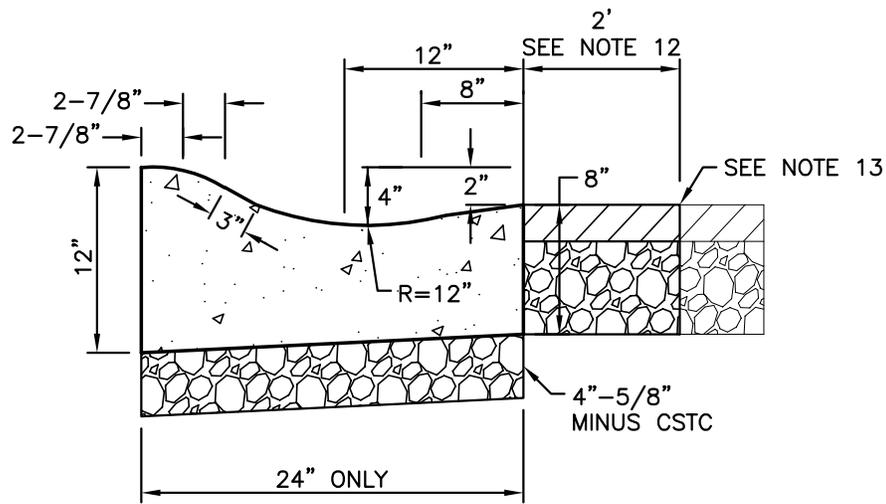
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**CONCRETE CURB
AND GUTTER**

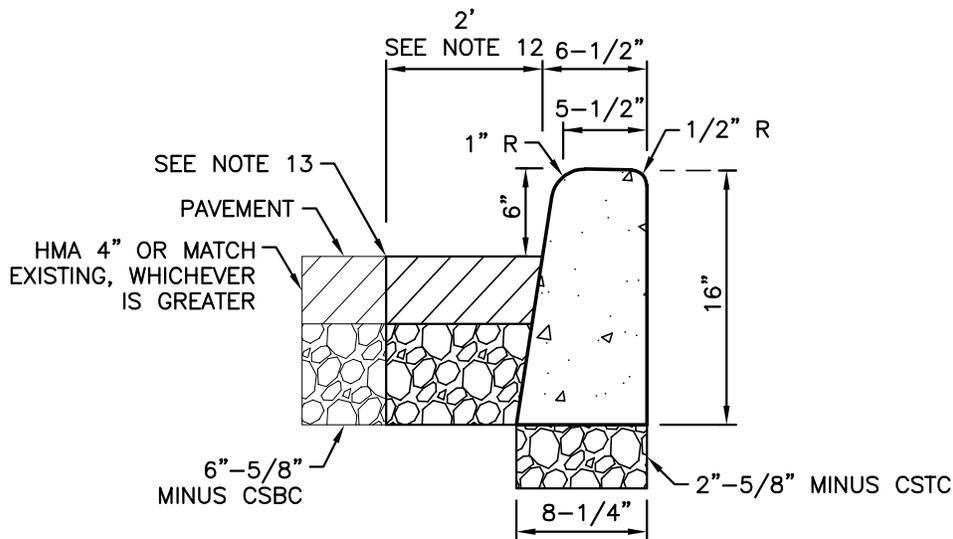
APPROVED BY: R. ENGLISH

REVISION DATE
OCTOBER 2018

STANDARD
DETAIL
TR-520



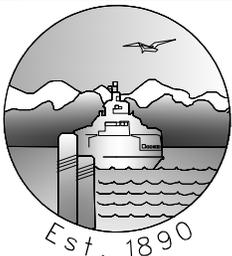
ROLLED CURB/GUTTER (SEE NOTE 10)
(NOT TO SCALE)



CONCRETE VERTICAL CURB (SEE NOTE 11)
(NOT TO SCALE)

NOTES:

1. CITY INSPECTION REQUIRED ON FORM WORK PRIOR TO POUR.
2. FORMS SHALL BE TRUE TO LINE AND GRADE AND SECURELY STAKED.
3. EXPANSION JOINTS SHALL BE PLACED ADJACENT TO ANY STRUCTURE.
4. EXPANSION JOINTS SHALL BE EVERY 10 FEET.
5. EXPANSION JOINTS SHALL HAVE 1/2" TO 5/8" WIDE PREMOLDED JOINT FILLER.
6. CONCRETE SHALL BE CLASS 3000.
7. FINISH SHALL BE LIGHT BROOM.
8. CURB SHALL BE SPRAYED WITH CLEAR CURING COMPOUND OR SHALL BE COVERED AND KEPT MOIST FOR 72 HOURS.
9. REMOVAL/REPLACEMENT OF CONCRETE CURB SHALL BE FROM EXPANSION TO EXPANSION JOINT, UNLESS OTHERWISE DIRECTED BY CITY ENGINEER.
10. FOR CUL-DE-SAC AREAS AND TO REPAIR LIKE CURBING ONLY, AND/OR AS APPROVED BY CITY ENGINEER.
11. FOR REPLACEMENT OF EXISTING VERTICAL CURBING ONLY AND/OR AS APPROVED BY CITY ENGINEER.
12. A 2 FOOT MINIMUM ASPHALT SAWCUT MAY BE REQUIRED AS DETERMINED BY CITY ENGINEER.
13. ALL VERTICAL EDGES SHALL BE TACKED.
14. NATIVE AND GRAVEL SUBGRADE SHALL BE COMPACTED TO A MINIMUM OF 95% OF MAX. DENSITY.



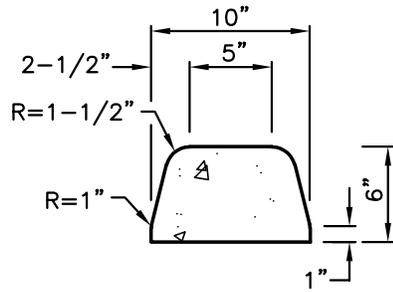
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**ROLLED AND VERTICAL
CURBS**

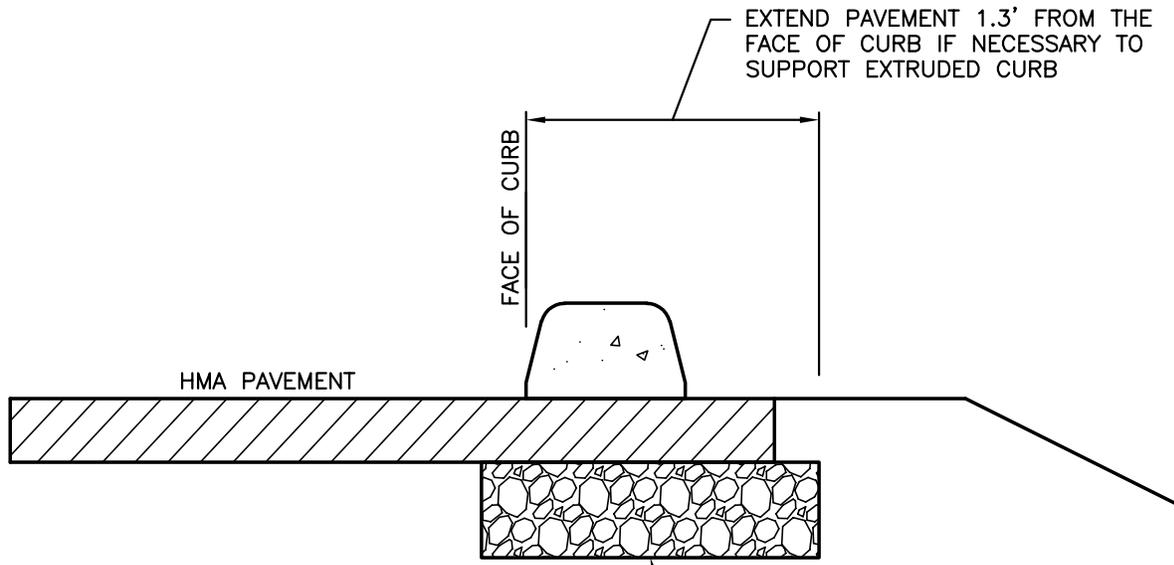
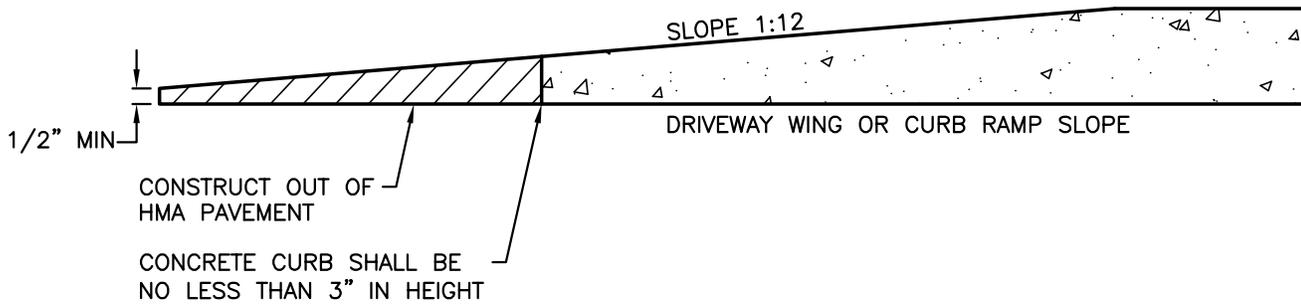
APPROVED BY: R. ENGLISH

REVISION DATE
OCTOBER 2018

STANDARD
DETAIL
TR-521

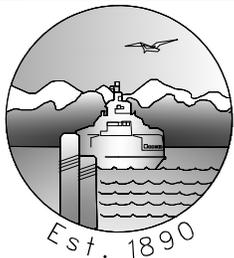


PLACE SCORE MARKS EVERY 10'



NOTES:

1. NOT TO BE USED IN RIGHT-OF-WAY EXCEPT TO REPAIR EXISTING EXTRUDED CURB, AS APPROVED BY CITY ENGINEER.
2. BONDING AGENT IS THE ONLY ATTACHMENT METHOD TO BE USED BETWEEN HMA AND EXTRUDED CURB.
3. CONCRETE SHALL BE AIR-ENTRAINED CLASS 3000 PER WSDOT STANDARD SPECIFICATIONS.
4. NATIVE AND GRAVEL SUBGRADE SHALL BE COMPACTED TO A MINIMUM OF 95% MAX DENSITY.



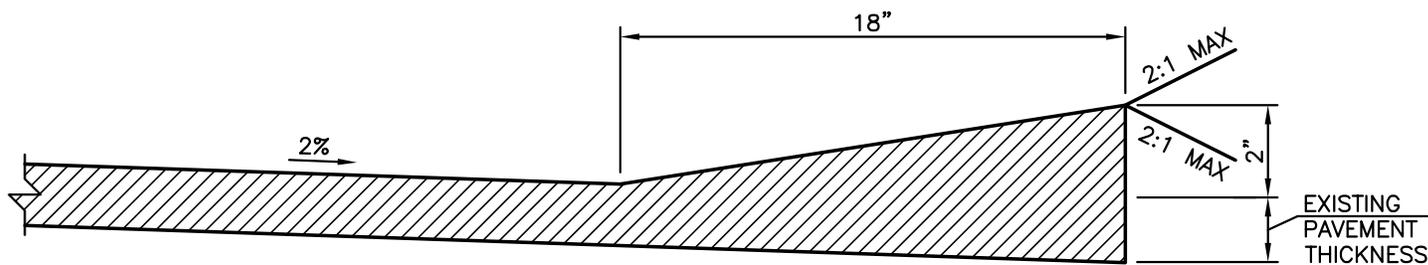
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**CONCRETE
EXTRUDED CURB**

APPROVED BY: R. ENGLISH

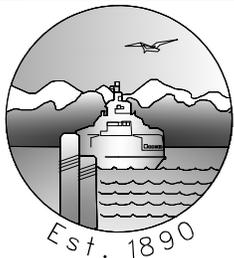
REVISION DATE
DECEMBER 2018

STANDARD
DETAIL
TR-522



NOTE:

IF A THICKENED EDGE IS ADDED TO EXISTING PAVEMENT OR APPLIED AFTER NEW PAVEMENT IS PLACED, THE ASPHALT SURFACE SHALL BE THOROUGHLY TACKED.



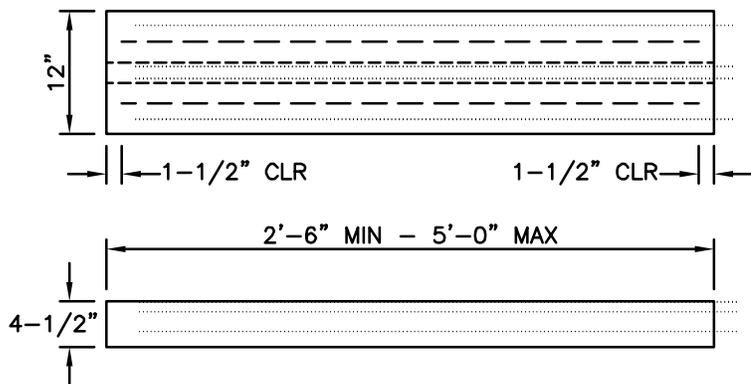
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

ASPHALT THICKENED EDGE

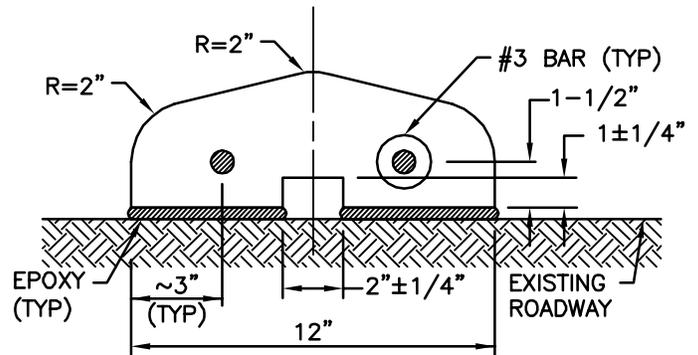
APPROVED BY: R. ENGLISH

REVISION DATE
OCTOBER 2019

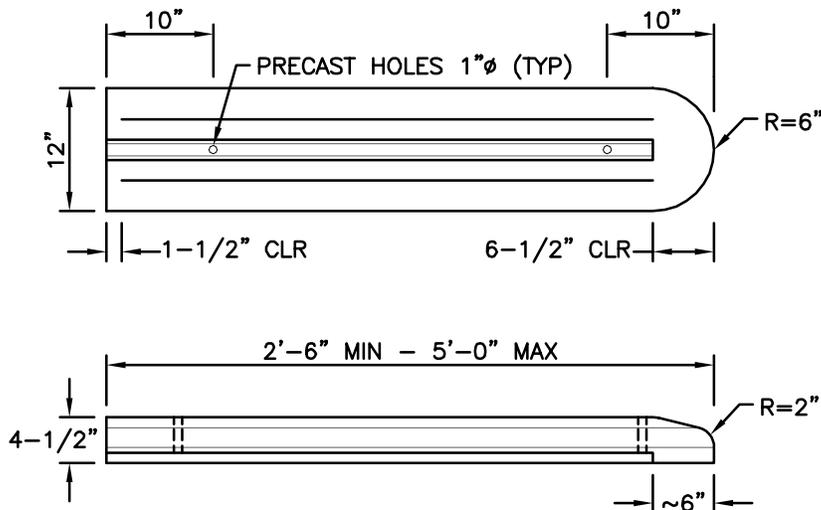
STANDARD
DETAIL
TR-523



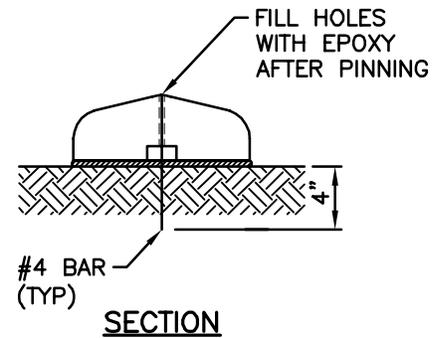
DUAL SLOPED BLOCK



SECTION



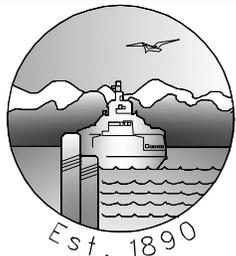
NOSING BLOCK



SECTION

NOTES:

1. 5' LONG STANDARD SECTIONS SHALL BE USED EXCLUSIVELY, EXCEPT TO ADJUST FOR FINAL LENGTH.
2. NOSING SHALL HAVE HOLES PRECAST AS SHOWN.
3. PRECAST TRAFFIC CURB SHALL BE INSTALLED WITH AN EAS-60 TWO PART EPOXY. EPOXY APPLICATION SHALL BE PER MANUFACTURER'S RECOMMENDATIONS.
4. EPOXY SHALL BE PLACED UNDER THE BEARING SURFACE OF THE PRECAST TRAFFIC CURB.
5. APPLY SUFFICIENT EPOXY TO ENSURE SQUEEZE-OUT OF 50% PER EACH SIDE OF EACH CURB SECTION.
6. DUAL FACED CURB MAY BE CONSTRUCTED BY USING (2) PRECAST MOUNTABLE CURBS (LONGITUDINAL HALVES).



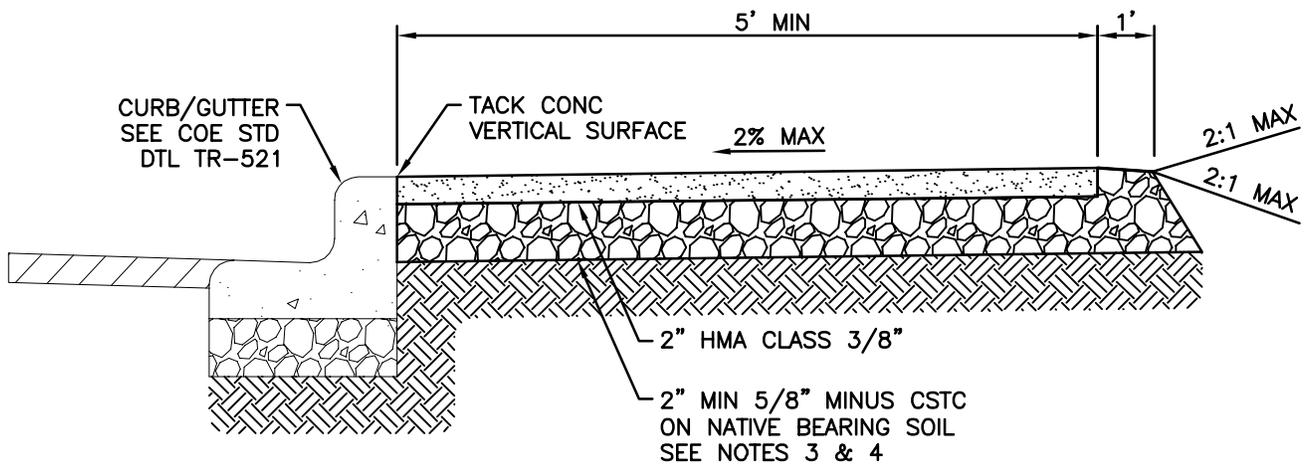
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**PRECAST CONCRETE
DUAL FACED SLOPED
MOUNTABLE CURB**

APPROVED BY: **R. ENGLISH**

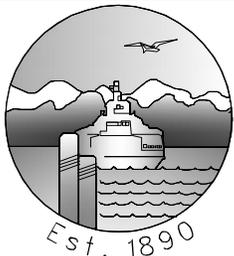
REVISION DATE
JANUARY 2018

STANDARD
DETAIL
TR-524



NOTES:

1. MINIMUM PAVEMENT COMPACTION – 91% OF RICE DENSITY.
2. SEE COE STD DTL TR-550 FOR POSSIBLE ROOT BARRIER INSTALLATION.
3. SUBGRADE AND 5/8" MINUS CSTC SHALL BE COMPACTED TO A MINIMUM OF 95% MAXIMUM DENSITY.
4. SUBGRADE INSPECTION REQUIRED PRIOR TO PLACING HMA.



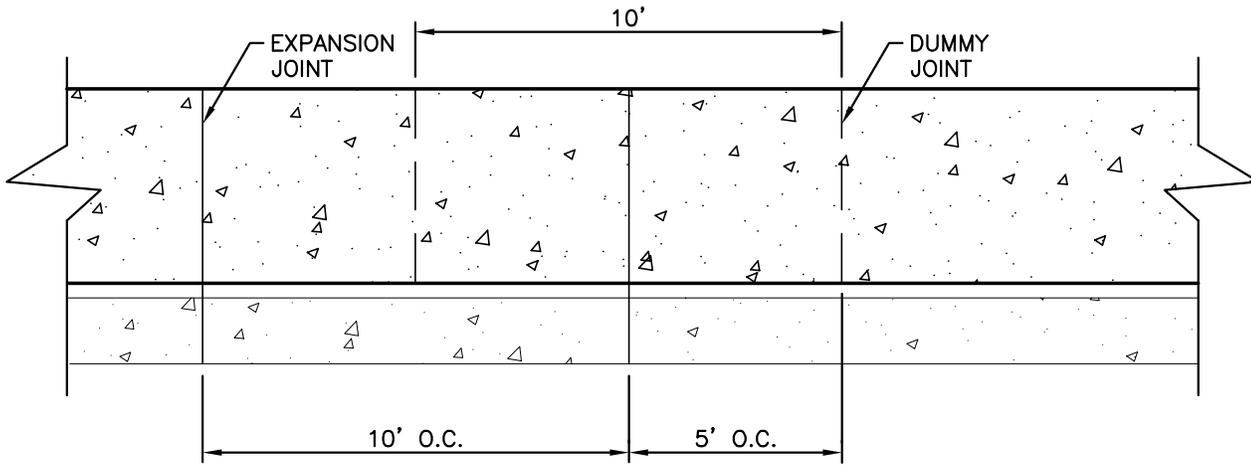
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

ASPHALT WALKWAY

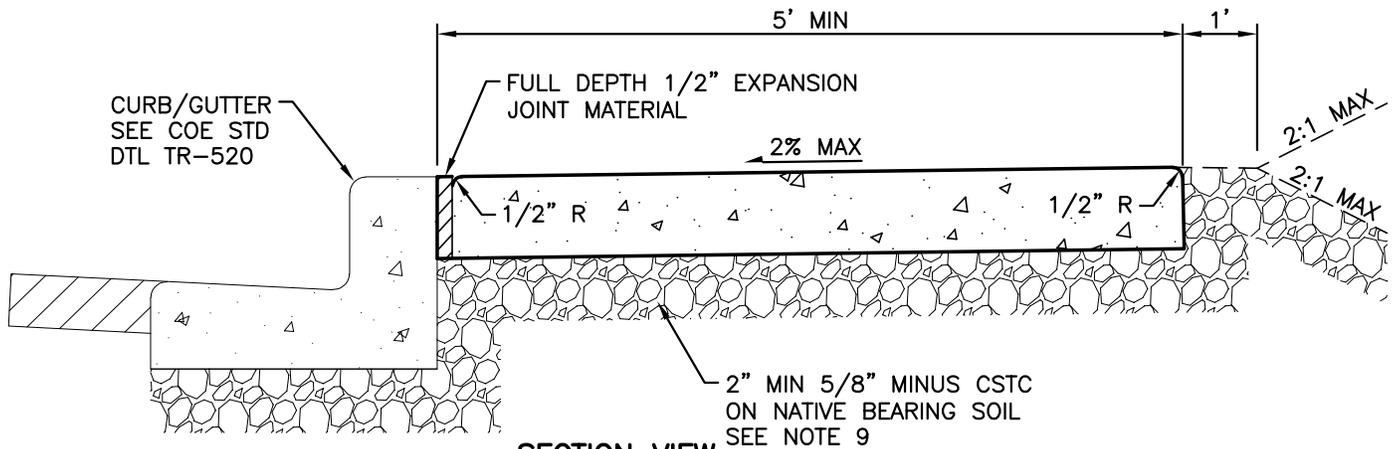
APPROVED BY: **R. ENGLISH**

REVISION DATE
JANUARY 2018

STANDARD
DETAIL
TR-530



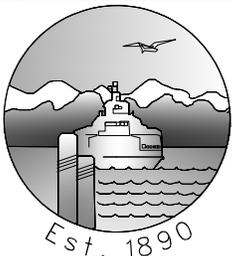
PLAN VIEW
(NOT TO SCALE)



SECTION VIEW
(NOT TO SCALE)

NOTES:

1. CITY INSPECTION REQUIRED ON FORM WORK PRIOR TO POUR.
2. CONCRETE SHALL BE CLASS 3000.
3. FINISH SHALL BE LIGHT BROOM.
4. SIDEWALK THICKNESS SHALL BE 4".
5. SIDEWALK THICKNESS AT DRIVEWAYS SHALL BE 6" THICK.
6. CURB AND GUTTER SHALL BE POURED SEPARATELY FROM SIDEWALK.
7. LIDS FOR JUNCTION BOXES AND UTILITY VAULTS SHALL BE NON-SKID AS SPECIFIED BY ENGINEER.
8. SEE COE STD DTL TR-550 FOR POSSIBLE ROOT BARRIER INSTALLATION.
9. SUBGRADE SHALL BE COMPACTED TO 95% MAXIMUM DENSITY.
10. ALL EDGES WITH EXPANSION JOINTS SHALL HAVE 1/2" RADIUS



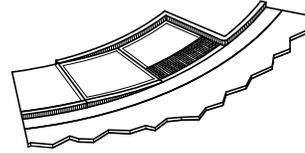
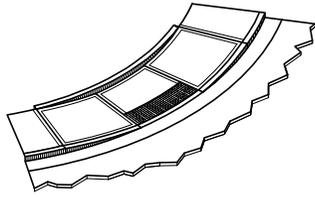
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

CONCRETE SIDEWALK

APPROVED BY: R. ENGLISH

REVISION DATE
JUNE 2019

STANDARD
DETAIL
TR-531

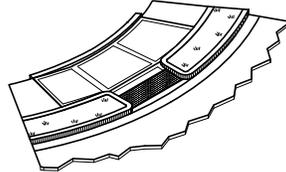


PARALLEL A

PARALLEL B

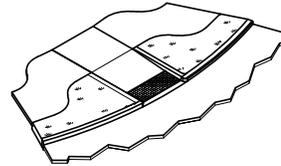
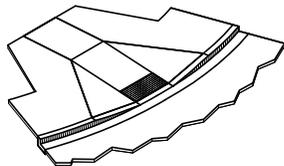
**PARALLEL CURB RAMP
WSDOT STANDARD PLAN F-40.12-03**

http://www.wsdot.wa.gov/publications/fulltext/Standards/english/PDF/f40.12-03_e.pdf



**COMBINATION CURB RAMP
WSDOT STANDARD PLAN F-40.14-03**

http://www.wsdot.wa.gov/publications/fulltext/Standards/english/PDF/f40.14-03_e.pdf

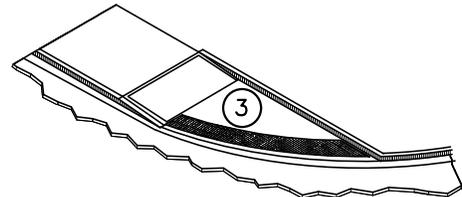
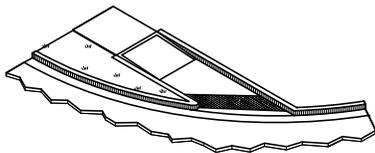


PERPENDICULAR A

PERPENDICULAR B

**PERPENDICULAR CURB RAMP
WSDOT STANDARD PLAN F-40.15-03**

http://www.wsdot.wa.gov/publications/fulltext/Standards/english/PDF/f40.15-03_e.pdf



SINGLE DIRECTION A

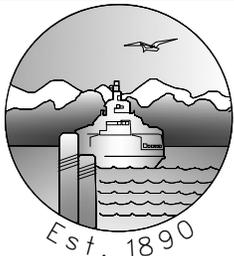
SINGLE DIRECTION A (MOD)

**SINGLE DIRECTION CURB RAMP
WSDOT STANDARD PLAN F-40.16-03**

http://www.wsdot.wa.gov/publications/fulltext/Standards/english/PDF/f40.16-03_e.pdf

NOTES:

- ① USE WSDOT DETAILS FOR HORIZONTAL LAYOUT, SLOPE, DETECTABLE WARNING SURFACE PLACEMENT. USE COE STD DTL TR-531 FOR ALL OTHER SPECIFICATIONS.
- ② SEE WSDOT DETECTABLE WARNING SURFACE STANDARD PLAN F-45.10-02 FOR DETECTABLE WARNING SURFACE INSTALLATION ON ALL RAMP TYPES
- ③ MUST INCLUDE A MIN. 4'X4' STREET LEVEL LANDING (MAX 2% SLOPE ALL DIRECTIONS).



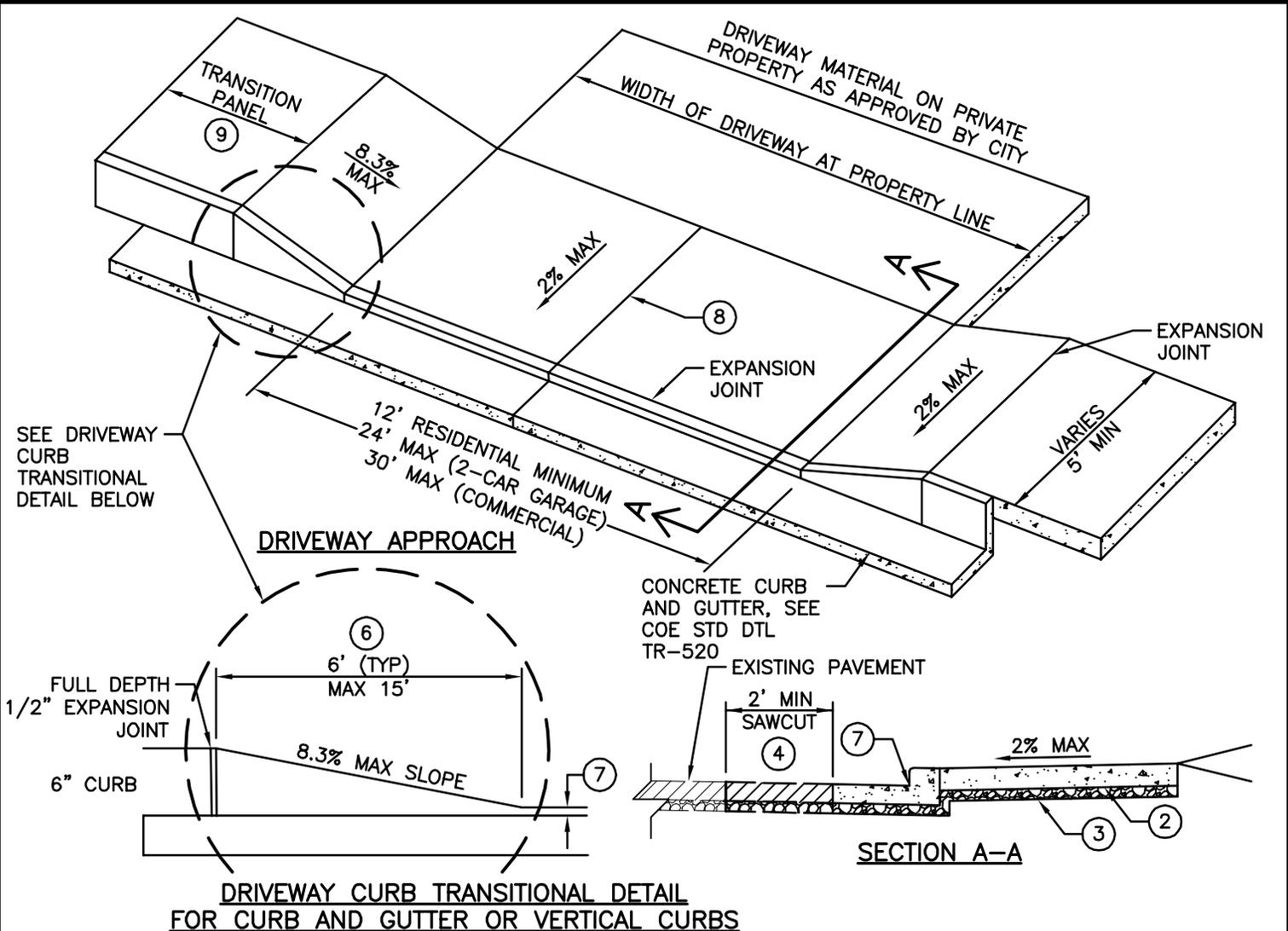
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

CURB RAMPS

APPROVED BY: R. ENGLISH

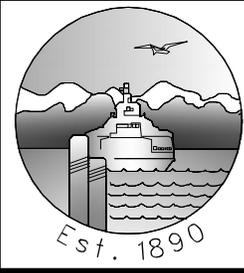
REVISION DATE
MARCH 2019

STANDARD
DETAIL
TR-540



NOTES:

- ① CURB AND GUTTER SHALL BE POURED SEPARATELY FROM THE SIDEWALK. CITY INSPECTION REQUIRED ON FORM WORK PRIOR TO POUR.
- ② DRIVEWAY APRON INCLUDING WING RAMPS SHALL BE A MINIMUM OF 6" THICK AND SHALL BE PLACED ON 2" OF 5/8" CSTC COMPACTED TO 95% MAXIMUM DENSITY.
- ③ SUBGRADE SHALL BE COMPACTED TO 95% MAXIMUM DENSITY.
- ④ 2' ASPHALT SAWCUT MAY BE REQUIRED WITH CURB/GUTTER INSTALLATION. REFER TO COE STD DTL TR-520.
- ⑤ CONCRETE SHALL BE CLASS 3000.
- ⑥ CURB TRANSITION SHOULD MAINTAIN A SLOPE NO GREATER THAN 8.3%. IF THE SLOPE REQUIREMENT CANNOT BE ACHIEVED, MAXIMUM CURB THE TRANSITION LENGTH SHALL BE 15- FEET.
- ⑦ MAINTAIN 1/2" LIP AT GUTTER.
- ⑧ IF DRIVEWAY WIDTH EXCEEDS 15', INSTALL A FULL DEPTH EXPANSION JOINT AT CENTER OF DRIVEWAY.
- ⑨ INSTALL MINIMUM 5-FT TRANSITION PANEL BETWEEN DRIVEWAYS AND WHEN CONNECTING TO EXISTING SIDEWALK.
- ⑩ FINISH SHALL BE LIGHT BROOM.



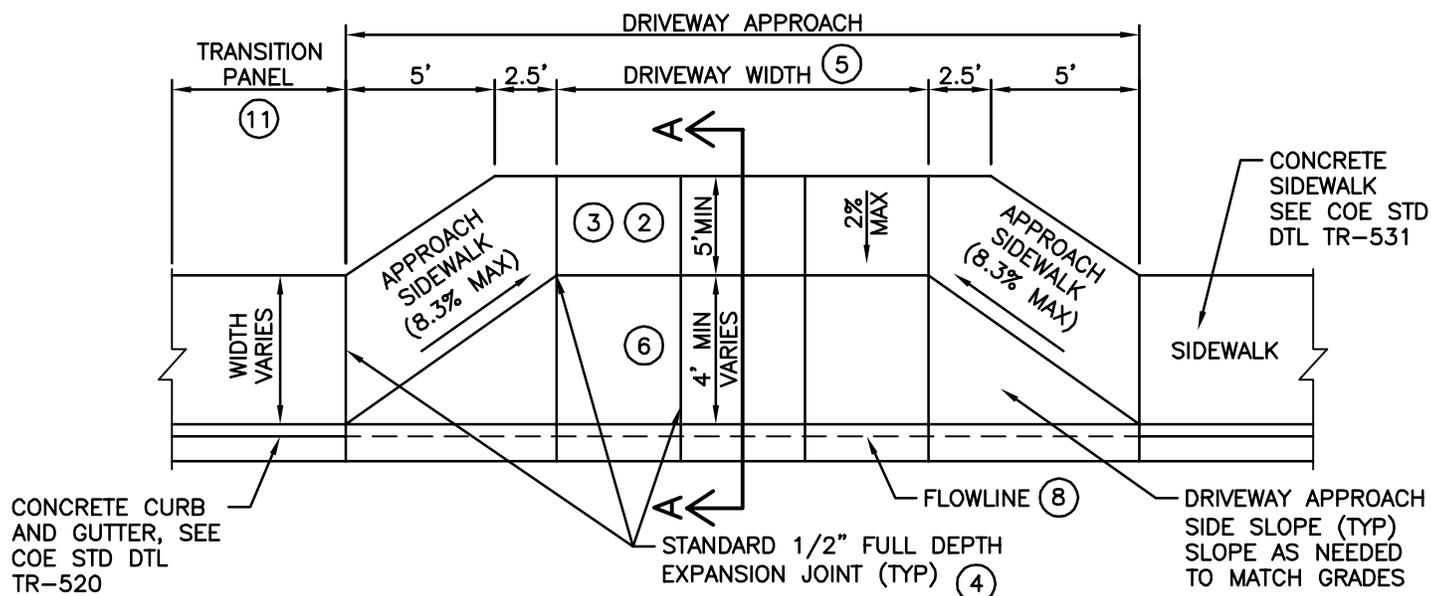
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**DRIVEWAY APPROACH
TYPE I**

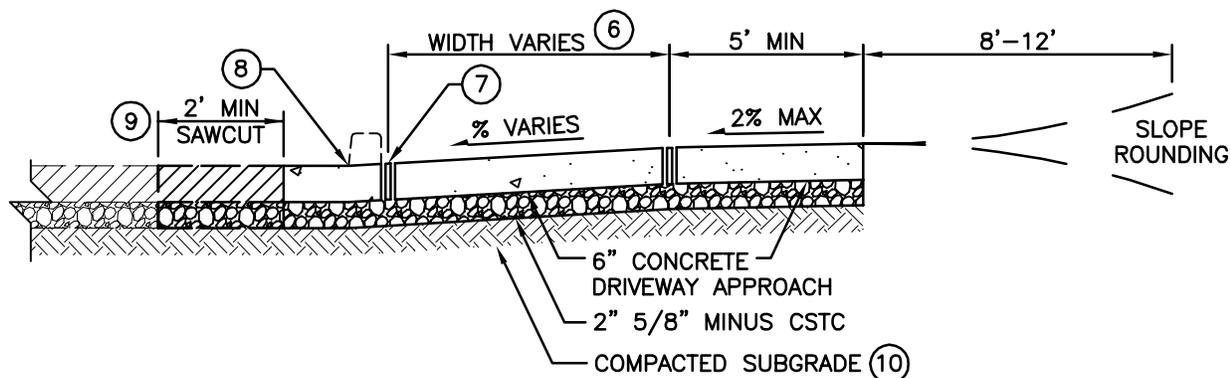
APPROVED BY: **R. ENGLISH**

REVISION DATE
JANUARY 2018

STANDARD
DETAIL
TR-541



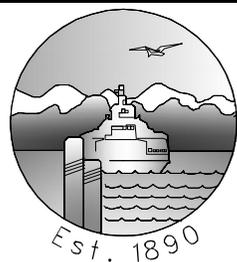
DRIVEWAY APPROACH



SECTION A-A

NOTES:

- ① CURB & GUTTER SHALL BE POURED SEPARATELY FROM SIDEWALK. CITY INSPECTION REQUIRED ON FORM WORK PRIOR TO POUR.
- ② CONCRETE SHALL BE CLASS 3000.
- ③ FINISH SHALL BE LIGHT BROOM.
- ④ FULL DEPTH 1/2" THRU EXPANSION JOINTS SHALL BE PLACED AT BACK, SIDES AND FRONT. MAXIMUM EXPANSION JOINT SPACING IS 10' CENTER TO CENTER. WHERE DRIVEWAY WIDTH IS LESS THAN 20', EXPANSION SHALL BE PLACED AT CENTER OF DRIVEWAY.
- ⑤ DRIVEWAY WIDTHS TO BE CONSISTENT WITH CITY STD DETAIL TR-541. DRIVEWAY WIDTH DOES NOT INCLUDE ADJACENT RAMPS.
- ⑥ A MINIMUM 4' RAMP TO BE INSTALLED UNLESS APPROVED OTHERWISE BY CITY ENGINEER.
- ⑦ FULL DEPTH 1/2" EXPANSION JOINT REQUIRED BETWEEN CURB AND DRIVEWAY APPROACH.
- ⑧ MAINTAIN 1/2" LIP AT GUTTER ADJACENT TO DRIVEWAY APPROACH.
- ⑨ 2' ASPHALT SAWCUT MAY BE REQUIRED WITH CURB/GUTTER INSTALLATION. REFER TO COE STD DTL TR-520.
- ⑩ SUBGRADE SHALL BE COMPACTED TO 95% MAXIMUM DENSITY
- ⑪ INSTALL MIN 5-FT TRANSITION PANEL BETWEEN DRIVEWAYS AND WHEN CONNECTING TO EXISTING SIDEWALK.



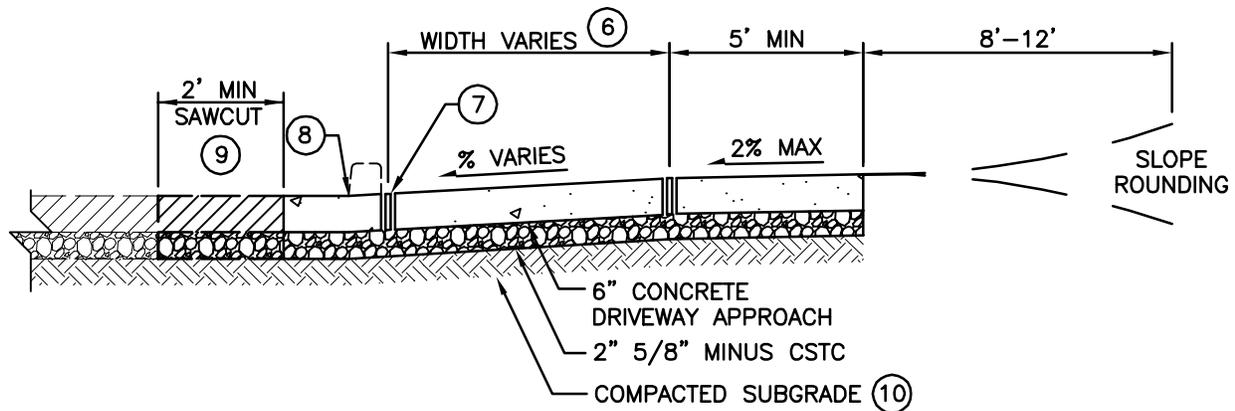
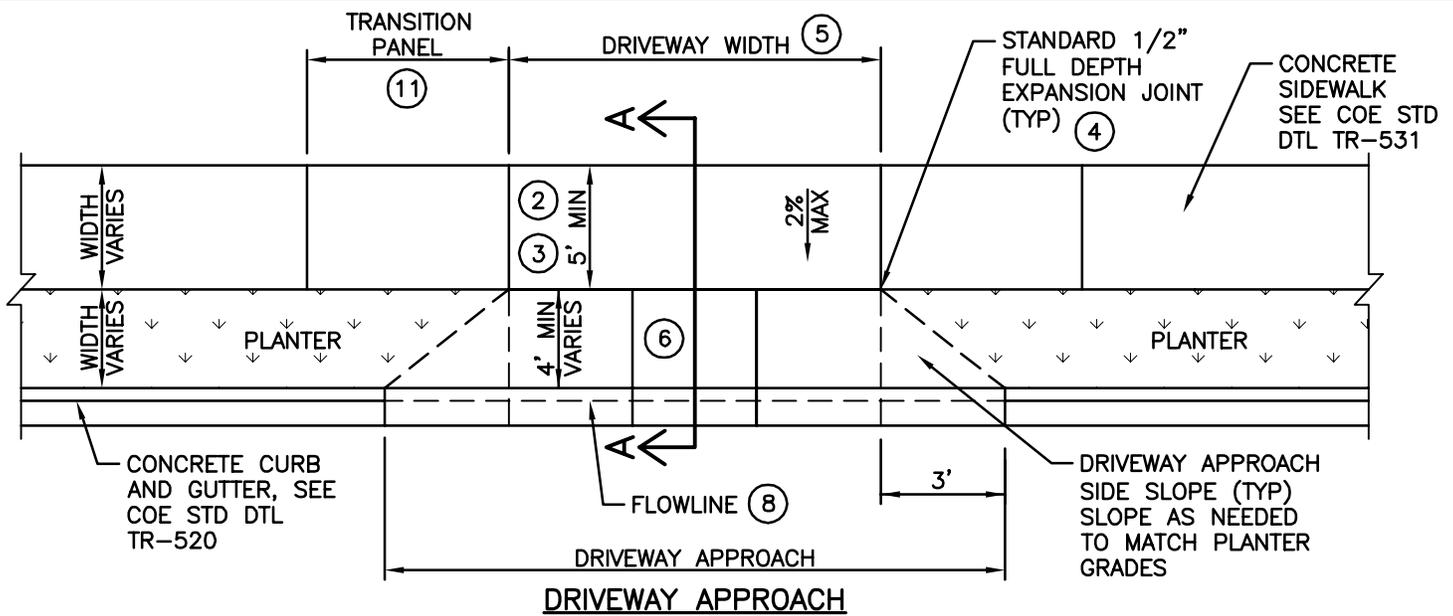
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**DRIVEWAY APPROACH
TYPE II**

APPROVED BY: **R. ENGLISH**

REVISION DATE
JANUARY 2018

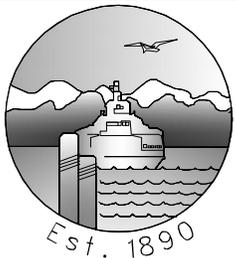
STANDARD
DETAIL
TR-542



SECTION A-A

NOTES:

- ① CURB AND GUTTER SHALL BE POURED SEPARATELY FROM SIDEWALK. CITY INSPECTION REQUIRED ON FORM WORK PRIOR TO POUR.
- ② CONCRETE SHALL BE CLASS 3000.
- ③ FINISH SHALL BE LIGHT BROOM.
- ④ FULL DEPTH 1/2" THRU EXPANSION JOINTS SHALL BE PLACED AT BACK, SIDES AND FRONT. MAXIMUM EXPANSION JOINT SPACING IS 10' CENTER TO CENTER. WHERE DRIVEWAY WIDTH IS LESS THAN 20', EXPANSION SHALL BE PLACED AT CENTER OF DRIVEWAY.
- ⑤ DRIVEWAY WIDTHS TO BE CONSISTENT WITH CITY STD DTL TR-541. DRIVEWAY WIDTH DOES NOT INCLUDE ADJACENT RAMPS.
- ⑥ A MINIMUM 4' RAMP TO BE INSTALLED UNLESS APPROVED OTHERWISE BY CITY ENGINEER.
- ⑦ FULL DEPTH 1/2" EXPANSION JOINT REQUIRED BETWEEN CURB AND DRIVEWAY APPROACH.
- ⑧ MAINTAIN 1/2" LIP AT GUTTER ADJACENT TO DRIVEWAY APPROACH.
- ⑨ 2' ASPHALT SAWCUT MAY BE REQUIRED WITH CURB/GUTTER INSTALLATION. REFER TO COE STD DTL TR-520.
- ⑩ SUBGRADE SHALL BE COMPACTED TO 95% MAXIMUM DENSITY.
- ⑪ INSTALL MINIMUM 5-FT TRANSITION PANEL BETWEEN DRIVEWAYS AND WHEN CONNECTING TO EXISTING SIDEWALK.



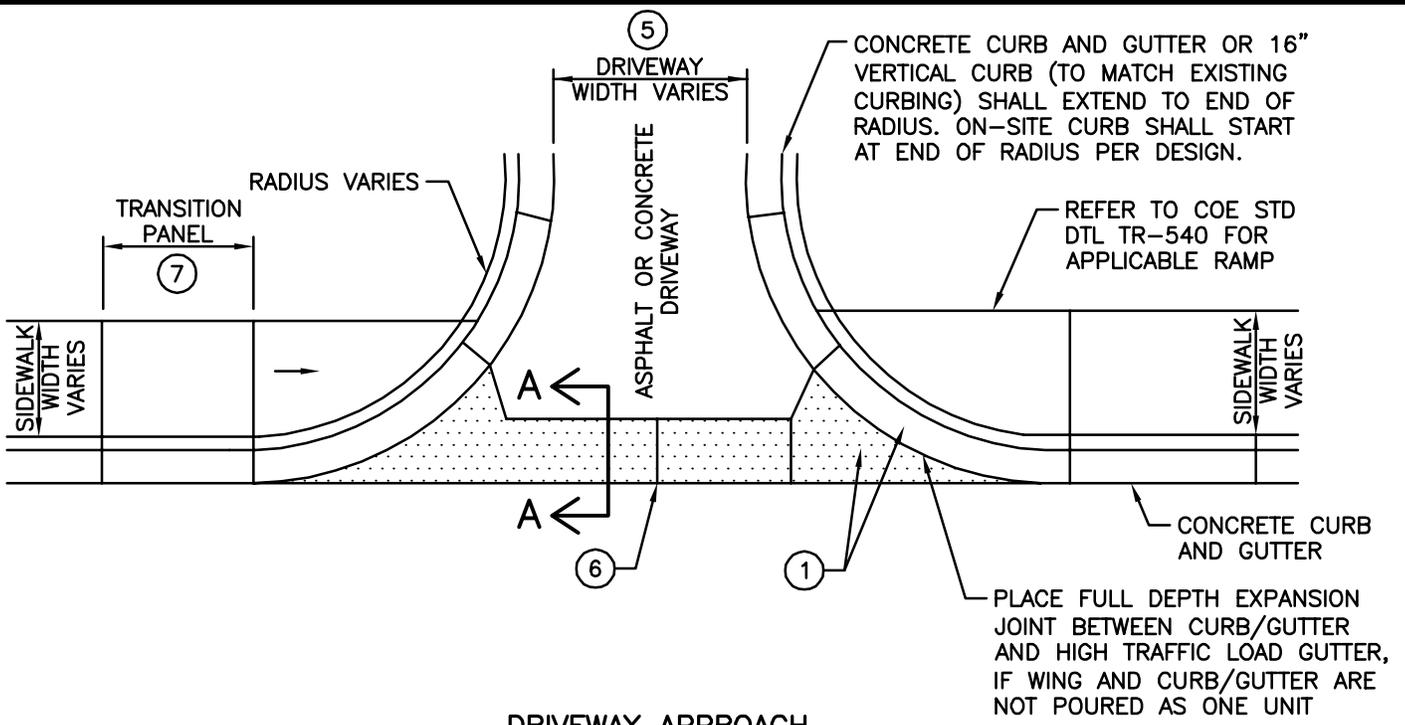
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**DRIVEWAY APPROACH
TYPE III**

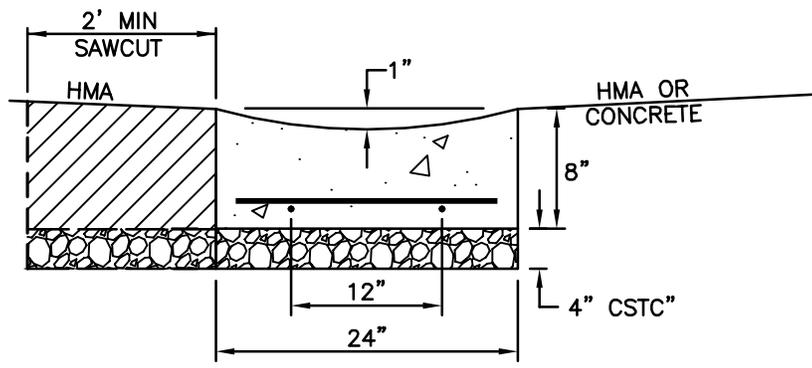
APPROVED BY: **R. ENGLISH**

REVISION DATE
JANUARY 2018

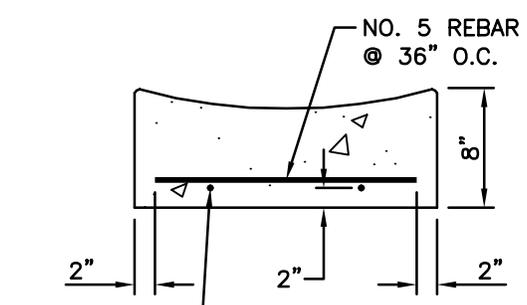
STANDARD
DETAIL
TR-543



DRIVEWAY APPROACH



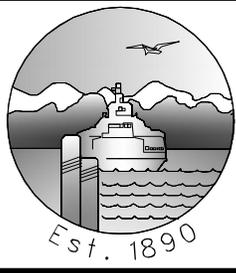
SECTION A-A



SECTION A-A REBAR DETAIL

NOTES:

- ① CURB/GUTTER AND WING FOR HIGH TRAFFIC LOAD GUTTER MAY BE POURED AS ONE UNIT. CITY INSPECTION REQUIRED ON FORM WORK PRIOR TO POUR.
- ② CONCRETE SHALL BE CLASS 3000.
- ③ FINISH SHALL BE LIGHT BROOM.
- ④ CITY ENGINEER SHALL DETERMINE APPLICABLE COMMERCIAL TYPE ENTRANCE BASED ON LOCATION AND OTHER SITE CONDITIONS.
- ⑤ DRIVEWAY WIDTHS TO BE CONSISTENT WITH CITY STD DETAIL TR-541. DRIVEWAY WIDTH DOES NOT INCLUDE RADIUS.
- ⑥ FULL DEPTH EXPANSION JOINTS SHALL BE PLACED IF WIDTH OF OPENING EXCEEDS 15'.
- ⑦ INSTALL MINIMUM 5-FT TRANSITION PANEL BETWEEN DRIVEWAYS AND WHEN CONNECTING TO EXISTING SIDEWALK.
- ⑧ REINFORCING STEEL SHALL BE GRADE 60.



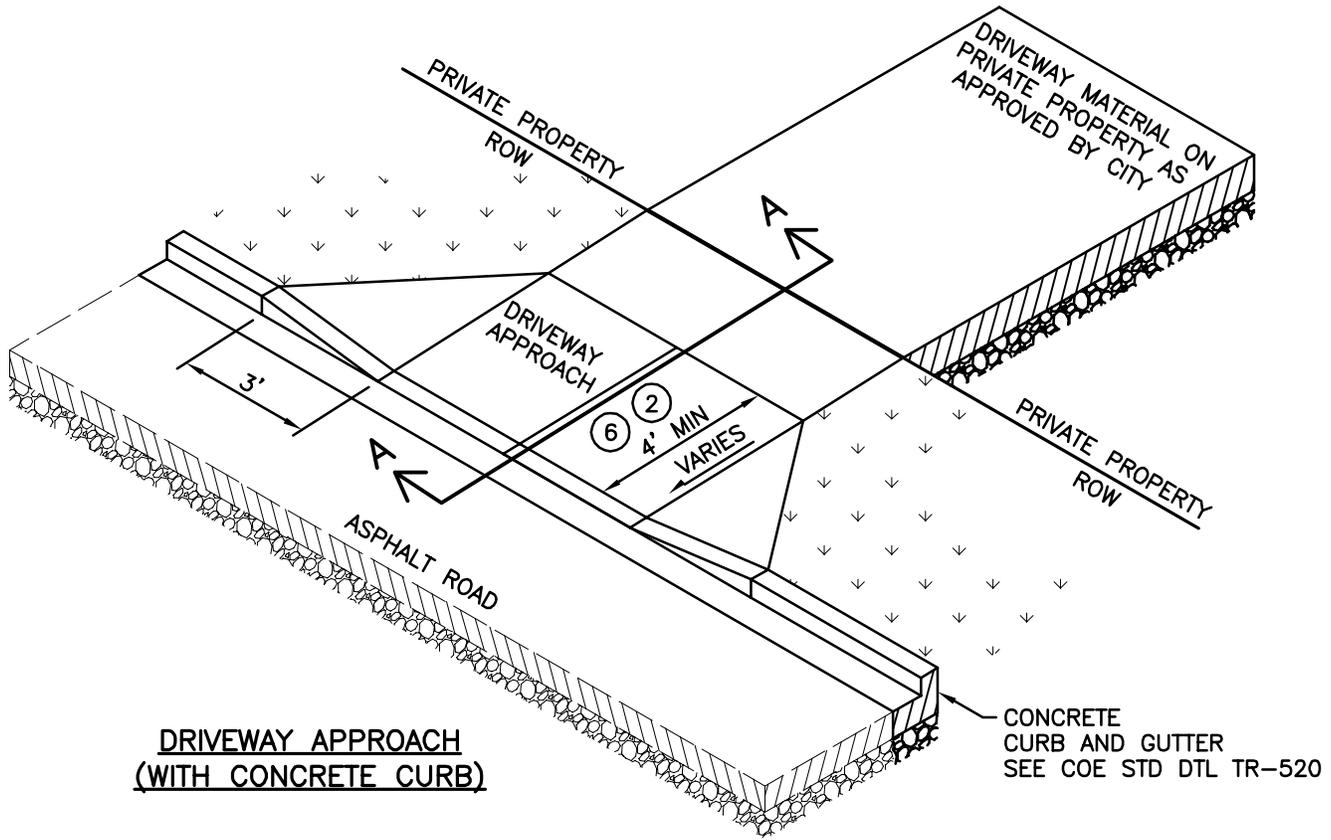
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**DRIVEWAY APPROACH
TYPE IV**

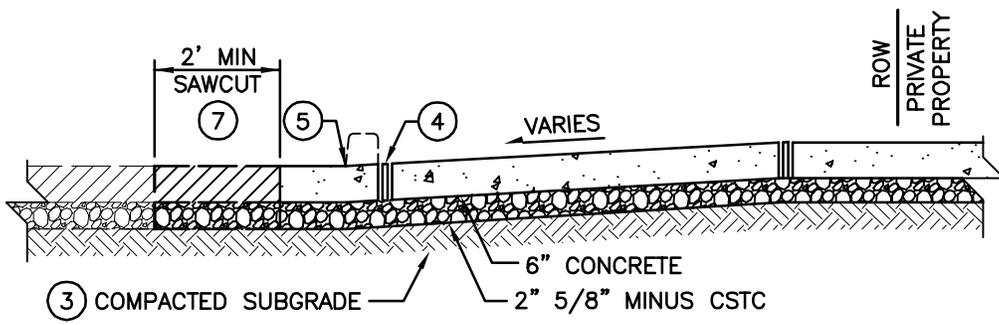
APPROVED BY: **R. ENGLISH**

REVISION DATE
JANUARY 2018

STANDARD
DETAIL
TR-544



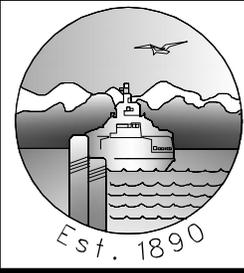
**DRIVEWAY APPROACH
(WITH CONCRETE CURB)**



SECTION A-A

NOTES:

- ① CITY INSPECTION REQUIRED ON FORM WORK PRIOR TO POUR.
- ② CONCRETE SHALL BE CLASS 3000.
- ③ SUBGRADE SHALL BE CSTC COMPACTED TO 95% MAXIMUM DENSITY.
- ④ FULL DEPTH 1/2" EXPANSION JOINT REQUIRED BETWEEN CURB AND DRIVEWAY APPROACH.
- ⑤ MAINTAIN 1/2" LIP AT GUTTER ADJACENT TO DRIVEWAY APPROACH.
- ⑥ IF DRIVEWAY APPROACH WIDTH EXCEEDS 15', INSTALL FULL DEPTH EXPANSION JOINT AT CENTER OF DRIVEWAY.
- ⑦ 2' ASPHALT SAWCUT MAY BE REQUIRED WITH CURB/GUTTER INSTALLATION. REFER TO COE STD DTL TR-520.



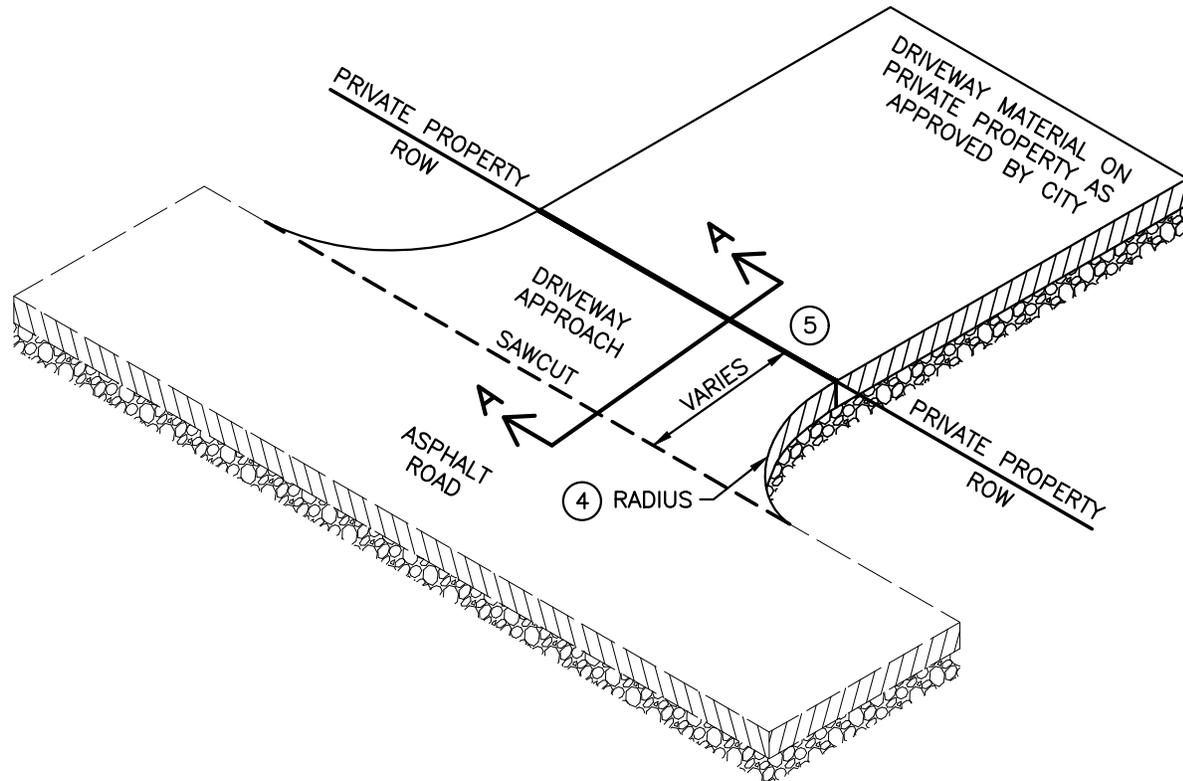
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**DRIVEWAY APPROACH
TYPE V**

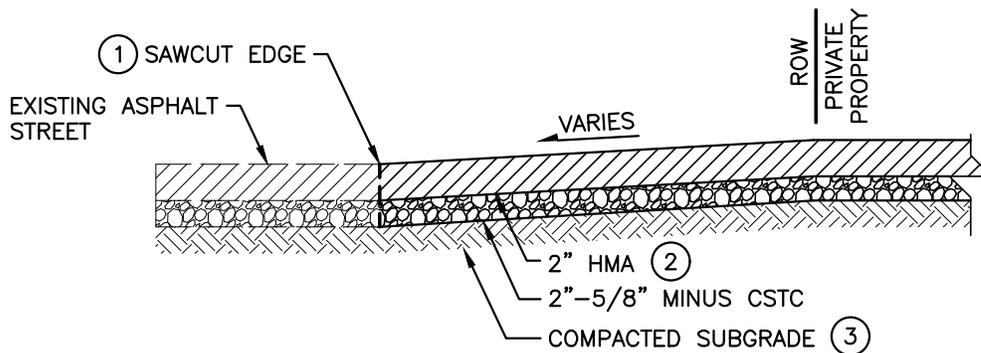
APPROVED BY: **R. ENGLISH**

REVISION DATE
JANUARY 2018

STANDARD
DETAIL
TR-545



DRIVEWAY APPROACH (WITHOUT CURB)



SECTION A-A

NOTES:

- ① SAWCUT AND USE TACK COAT BETWEEN ROAD TO DRIVEWAY APPROACH JOINT. SEAL JOINT WITH AR4000 SEALER.
- ② DRIVEWAY APPROACH TO BE 4" HMA CLASS 1/2" PG 58H-22.
- ③ SUBGRADE SHALL BE COMPACTED TO 95% MAXIMUM DENSITY.
- ④ 10' EDGE RADIUS.
- ⑤ HMA SHALL BE PLACED FROM EDGE OF EXISTING STREET, A MINIMUM DISTANCE OF 20-FT OR TO PROPERTY LINE, WHICHEVER IS GREATER.



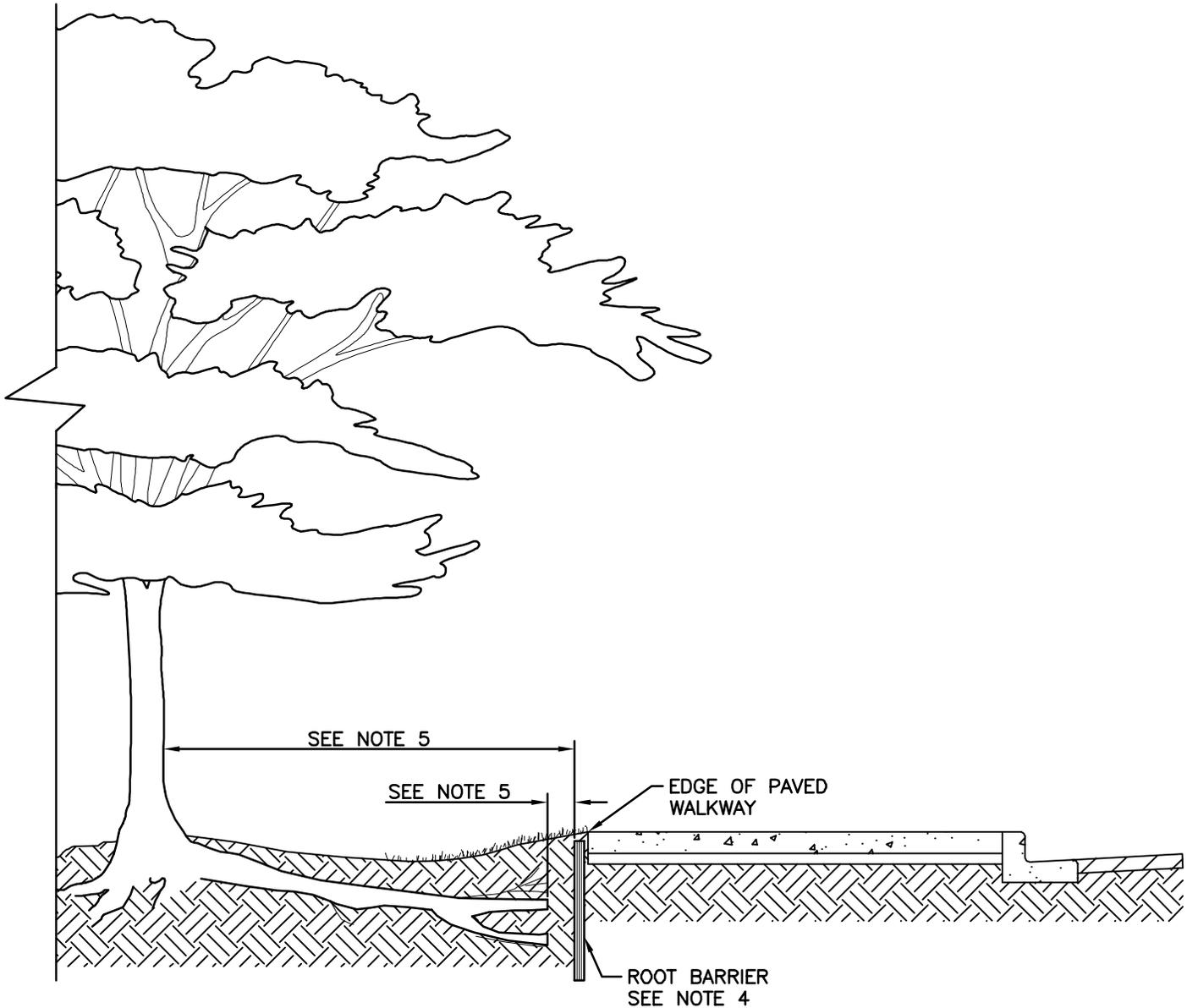
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**DRIVEWAY APPROACH
TYPE VI**

APPROVED BY: R. ENGLISH

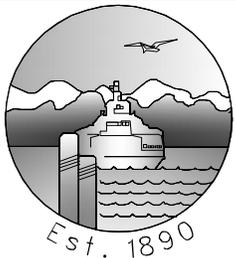
REVISION DATE
FEBRUARY 2020

STANDARD
DETAIL
TR-546



NOTES:

1. ALL ROOT PRUNING SHALL BE PERFORMED PER THE STANDARDS OF ANSI A300.
2. ALL ROOTS OVER 1" IN DIAMETER THAT HAVE BEEN EXPOSED AND DAMAGED DURING CONSTRUCTION ACTIVITIES SHALL BE REMOVED. MAKE A CLEAN, STRAIGHT CUT TO REMOVE ENTIRE DAMAGED PORTION OF ROOT.
3. ALL EXPOSED ROOTS SHALL BE TEMPORARILY COVERED WITH DAMP BURLAP TO PREVENT DRYING AND DAMAGE.
4. ROOT BARRIER SHALL BE A MINIMUM OF 18" DEEP AND SHALL BE MADE OF BIO-BARRIER TRIFLURALIN, AS ROOT CONTROL CHEMICAL OR APPROVED EQUAL. SET 1/2" BELOW FINISHED GRADE. CENTER ROOT BARRIER ON TRUNK OF TREE.
5. DISTANCE BETWEEN TREE AND ROOT BARRIER TO BE DETERMINED BY ARBORIST. MINIMUM DISTANCE IS 3 TIMES THE TRUNK CALIPER. GAP BETWEEN ROOTS AND ROOT BARRIER ALSO TO BE DETERMINED BY ARBORIST.



CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**ROOT BARRIER FOR
CONCRETE/PAVED
WALKWAY**

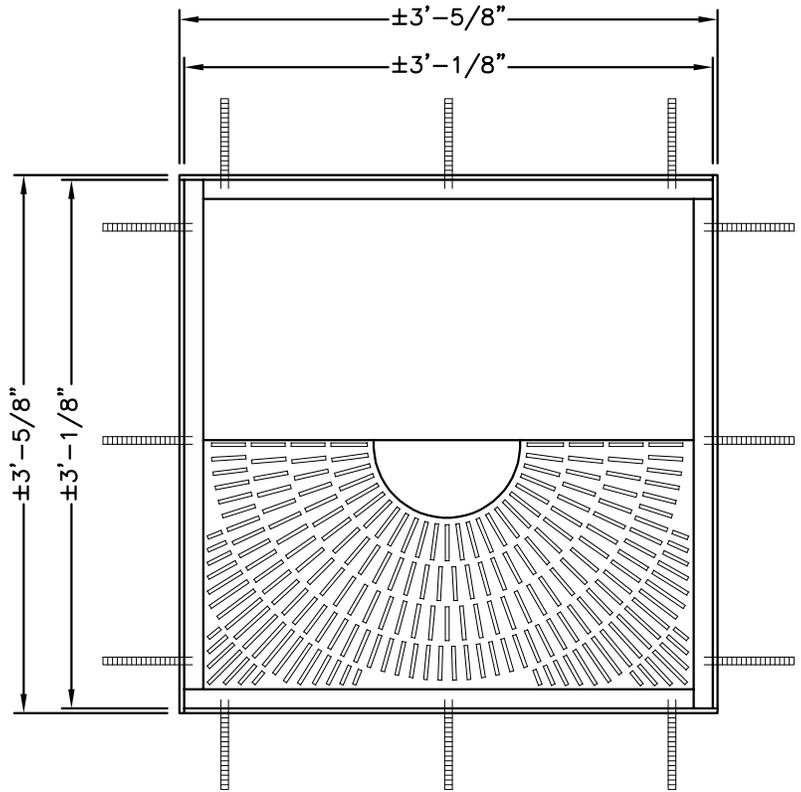
APPROVED BY: **R. ENGLISH**

REVISION DATE
JANUARY 2018

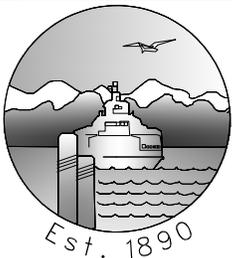
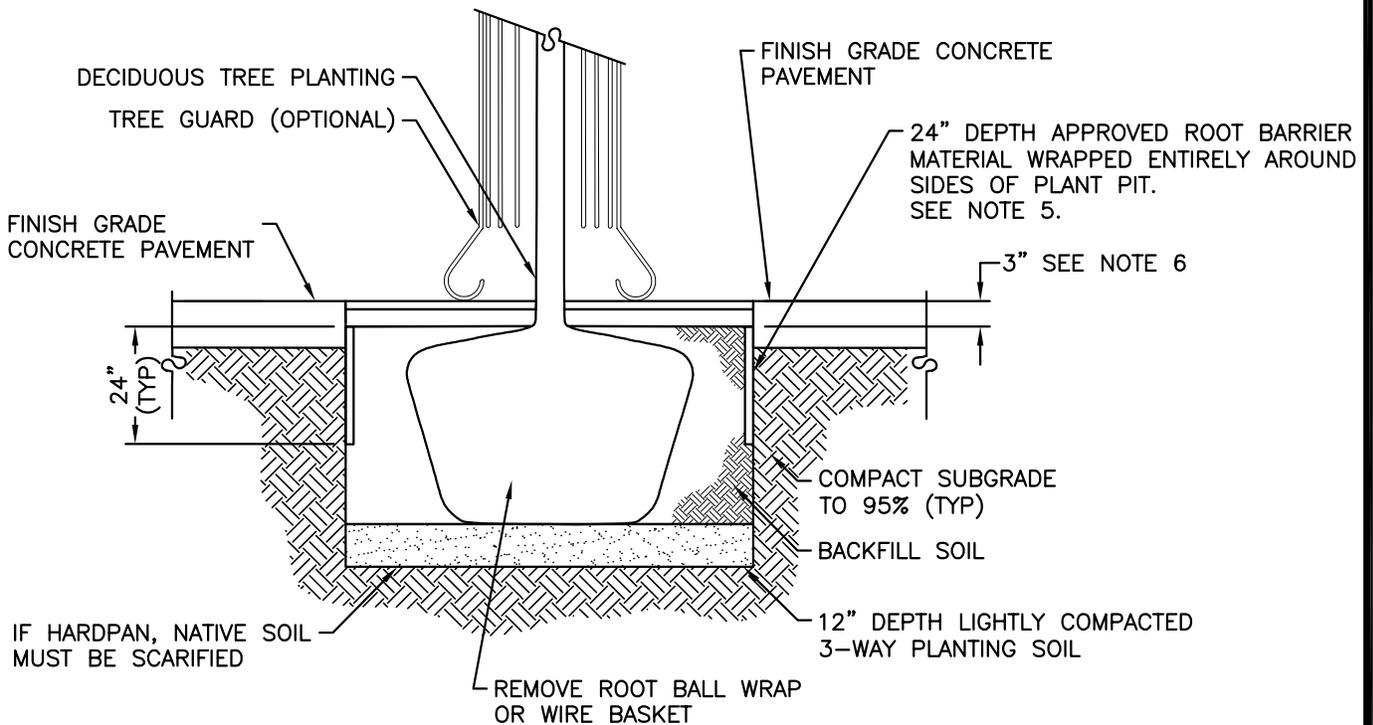
STANDARD
DETAIL
TR-550

NOTES:

1. TREE GRATE SHALL BE URBAN ACCESSORIES MODEL KIVA 3'x3' OR EQUIVALENT. TREE GRATE FRAME SHALL BE URBAN ACCESSORIES MODEL TYPE "S" OR EQUIVALENT.
2. SLOTS IN GRATE SHALL BE NO GREATER THAN 1/2" IN ONE DIRECTION.
3. HOLES (SQUARE OR ROUND), SHALL BE NO GREATER THAN 1/2" IN DIAMETER.
4. WITH ELONGATED OPENINGS, LONG DIMENSIONS SHOULD BE PERPENDICULAR TO THE DOMINANT DIRECTION OF TRAVEL.
5. ROOT BARRIER SHALL BE 24" DEEP AND SHALL BE MADE OF BIO-BARRIER TRIFLURALIN, AS ROOT CONTROL CHEMICAL OR APPROVED EQUAL.
6. BACKFILL SOIL FINAL GRADE SHALL NOT BE HIGHER THAN THE TREE ROOT FLARE. ROOT FLARE SHALL BE SET 3" BELOW THE TOP OF GRATE.



**ADA STANDARD
TREE GRATE
(SEE NOTES 1-4)**



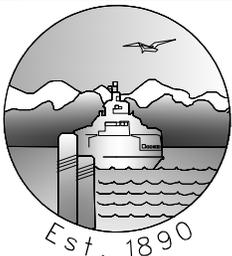
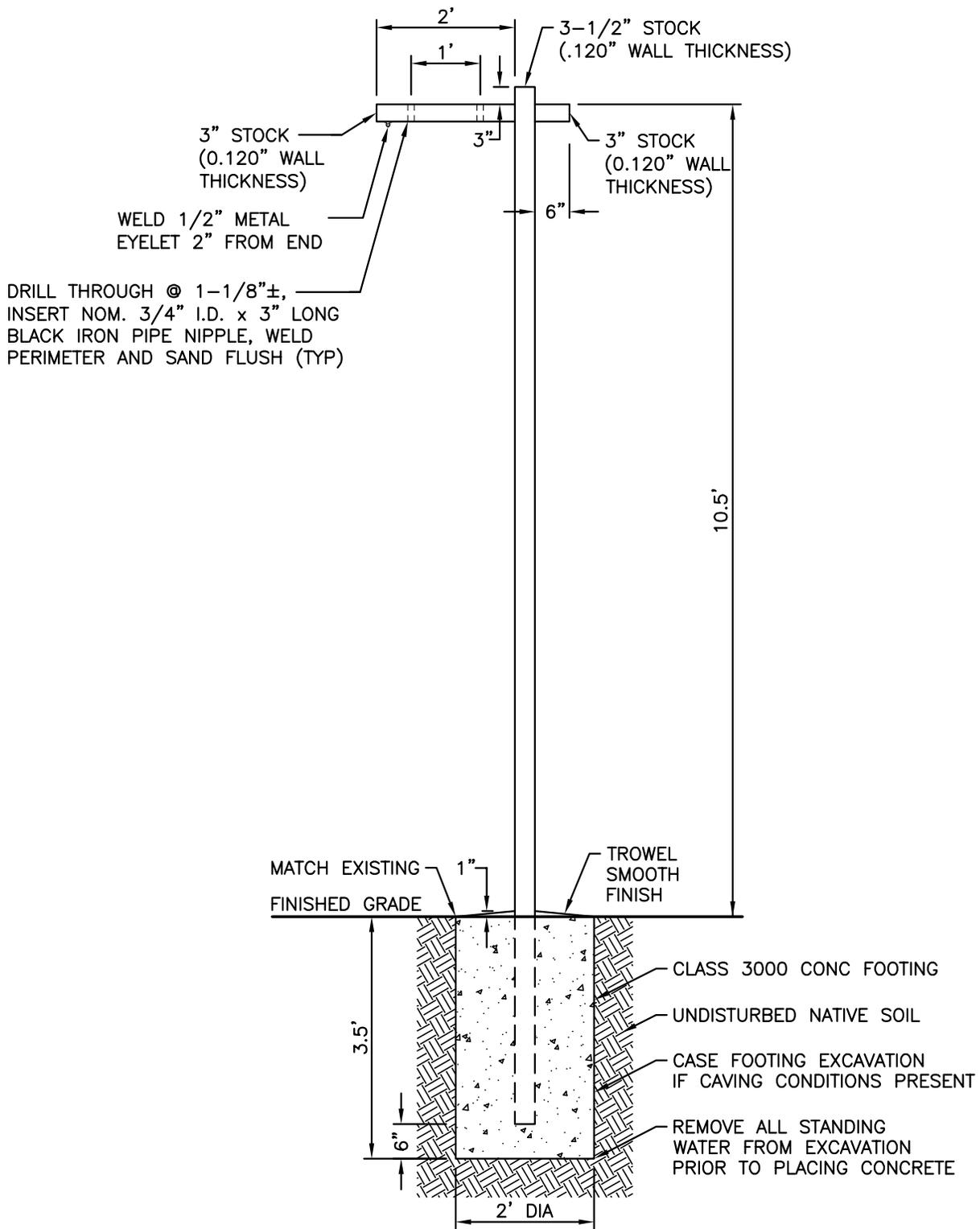
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

TREE GRATE

APPROVED BY: R. ENGLISH

REVISION DATE
APRIL 2019

STANDARD
DETAIL
TR-551



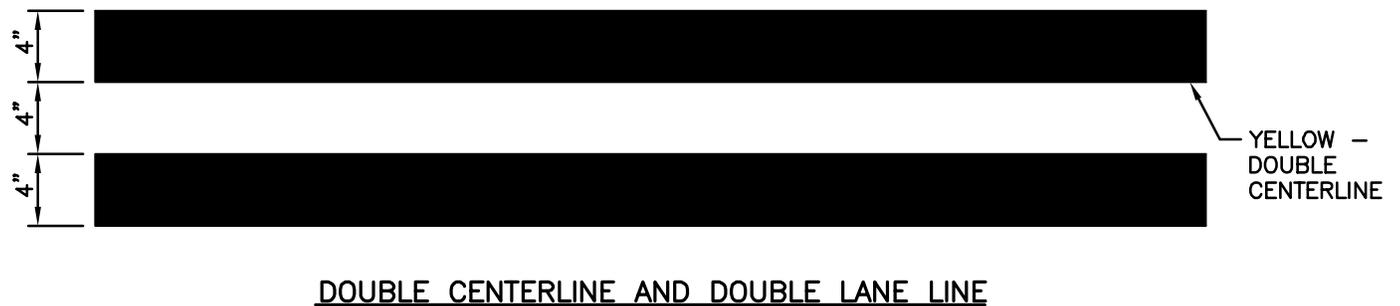
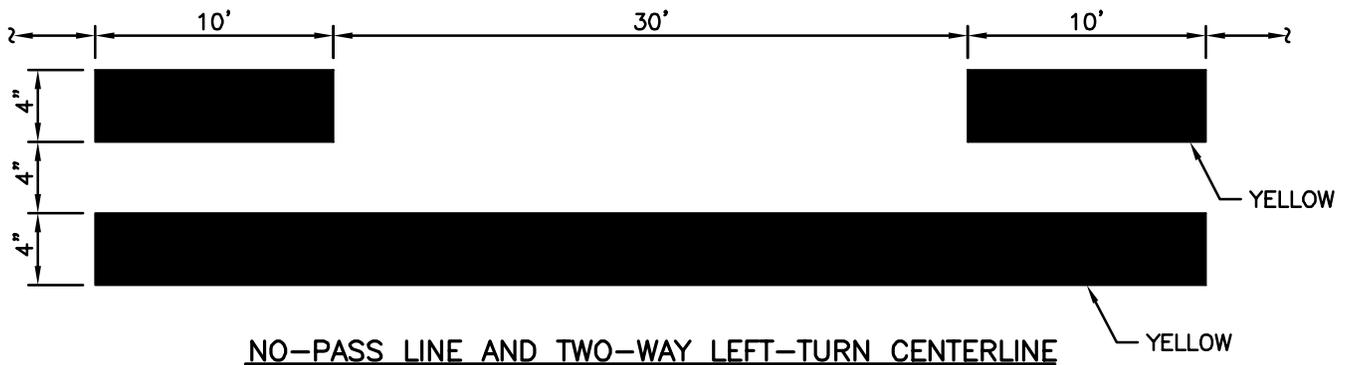
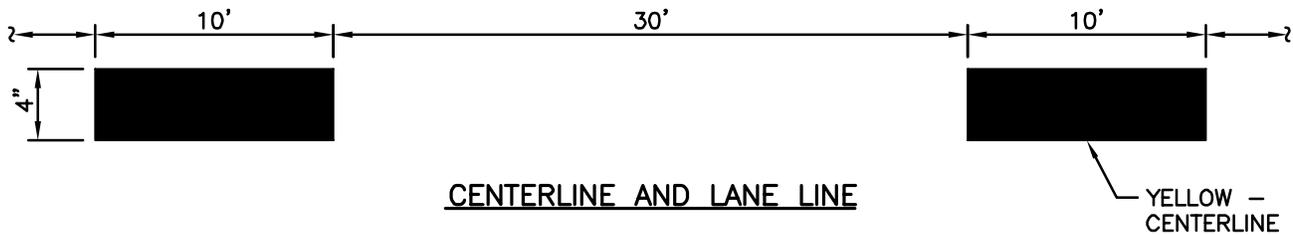
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**HANGING FLOWER
BASKET POLE**

APPROVED BY: R. ENGLISH

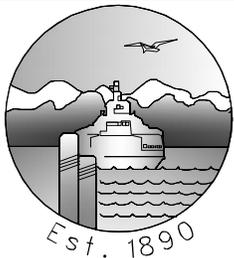
REVISION DATE
JUNE 2019

STANDARD
DETAIL
TR-552



NOTES:

1. FOG LINE SHALL BE WHITE ON THE RIGHT EDGE OF TRAVEL LANE.
2. ALL PAVEMENT MARKINGS SHALL CONFORM TO SECTION 8-22 PAVEMENT MARKING OF WSDOT/APWA STANDARD SPECS. STRIPES SHALL BE CONSTRUCTED WITH HIGH REFLECTIVE DOUBLE COAT WATER BORNE PAINT UNLESS OTHERWISE NOTED.



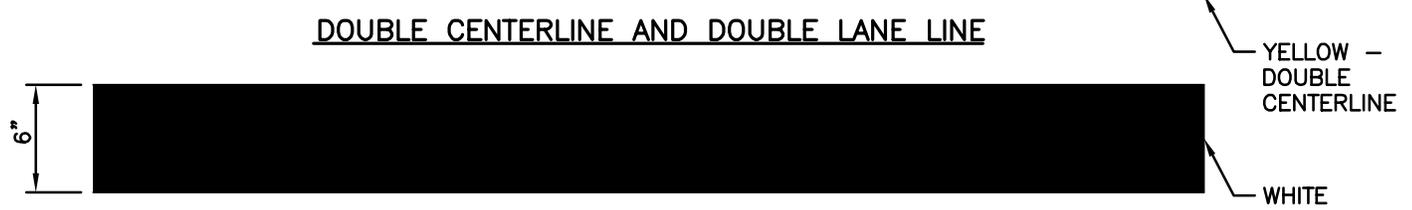
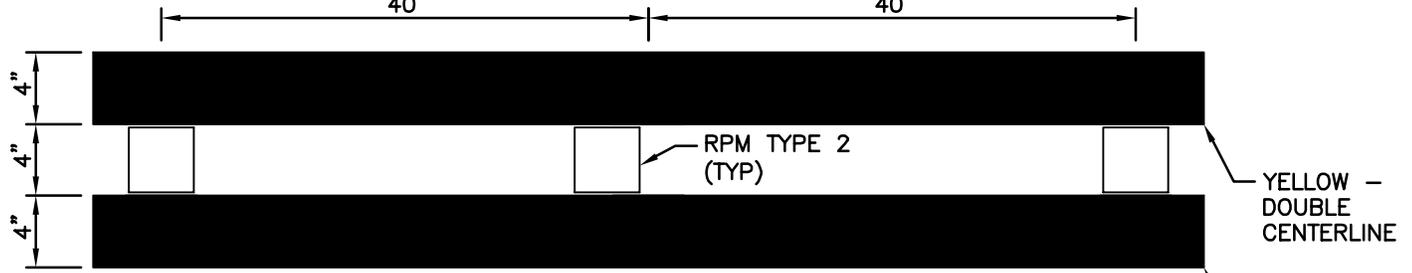
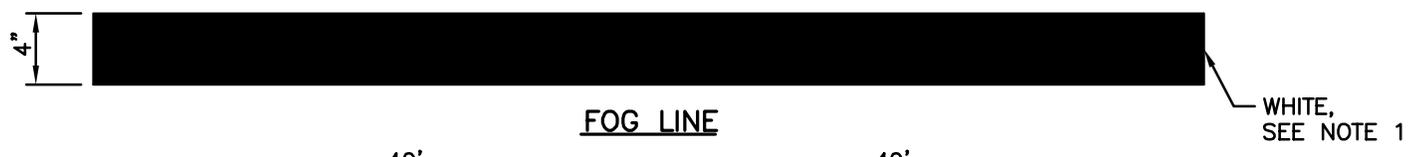
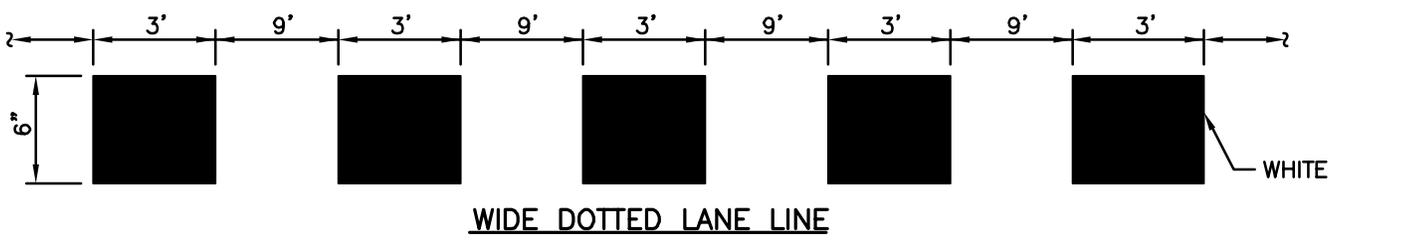
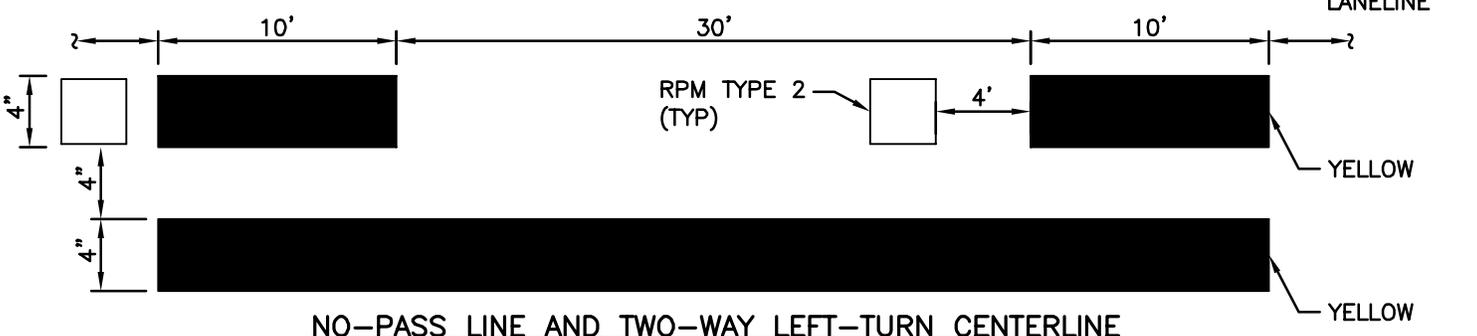
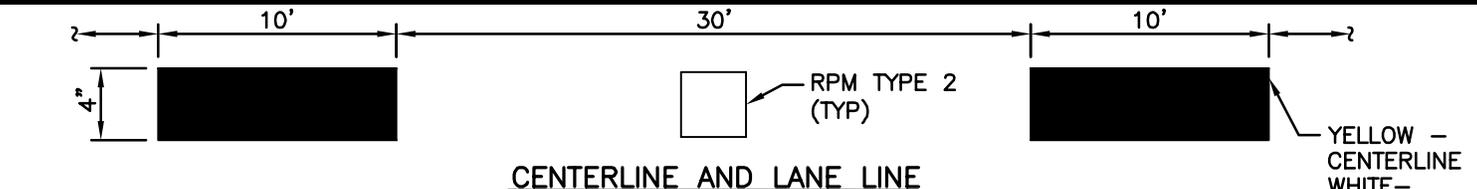
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**ROADWAY PAVEMENT
MARKINGS ON LOCAL
STREETS**

APPROVED BY: **R. ENGLISH**

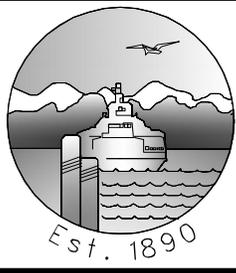
REVISION DATE
JANUARY 2018

STANDARD
DETAIL
TR-560



NOTES:

1. FOG LINE SHALL BE WHITE ON THE RIGHT EDGE OF TRAVEL LANE.
2. ALL PAVEMENT MARKINGS SHALL CONFORM TO SECTION 8-22 PAVEMENT MARKING OF WSDOT/APWA STANDARD SPECS. STRIPES SHALL BE CONSTRUCTED WITH HIGH REFLECTIVE DOUBLE COAT WATER BORNE PAINT UNLESS OTHERWISE NOTED.
3. RPM COLOR SHALL MATCH TO COLOR OF MARKING WHICH IT IS SUPPLEMENTING.



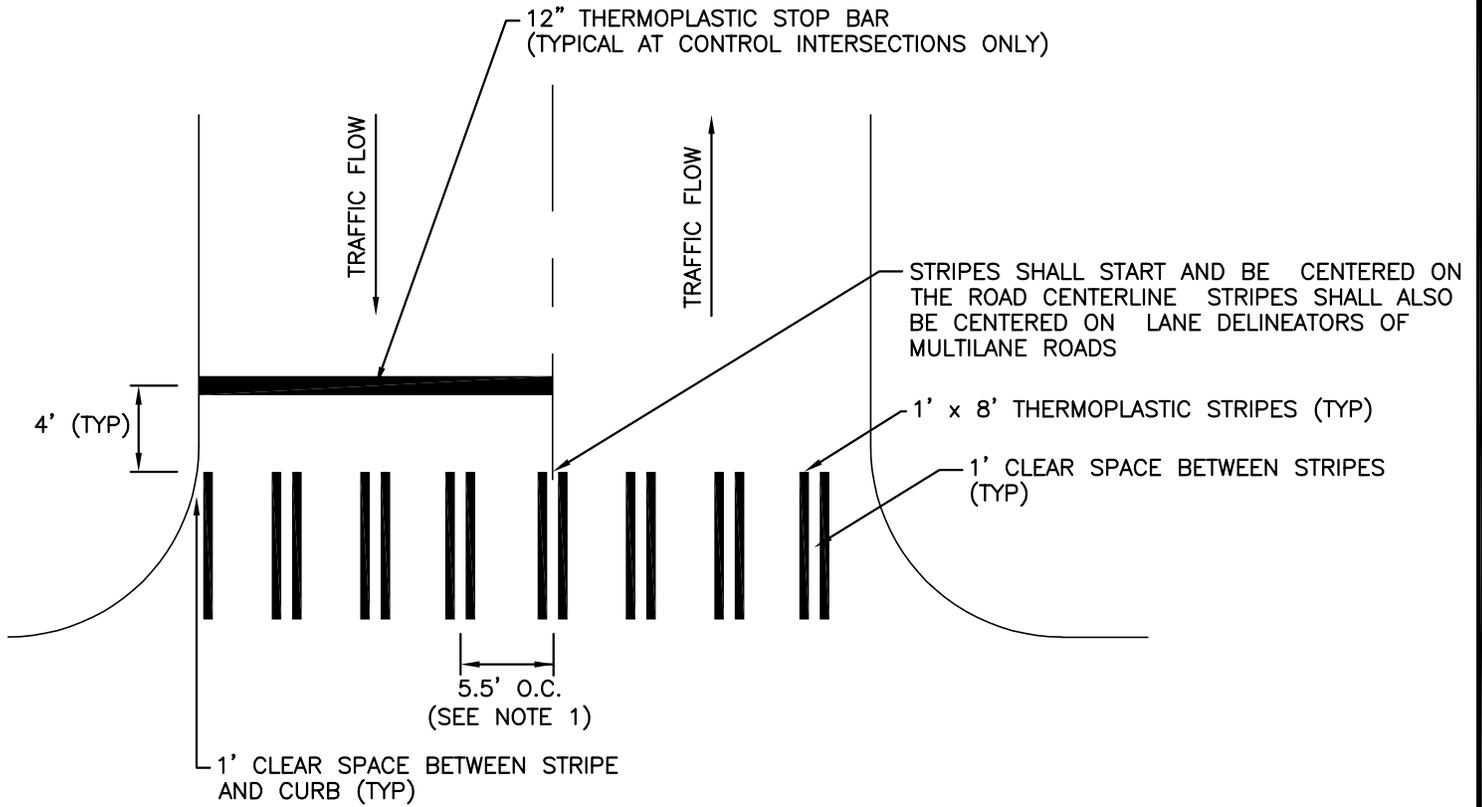
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**ROADWAY PAVEMENT
STRIPING FOR COLLECTOR
AND ARTERIAL STREETS**

APPROVED BY: R. ENGLISH

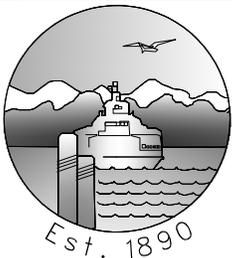
REVISION DATE
JANUARY 2018

STANDARD
DETAIL
TR-561



NOTES:

1. DIMENSION MAY VARY DEPENDING ON ROAD WIDTH (COORDINATE ACTUAL SPACING WITH ENGINEER)
2. MATERIAL SHALL BE HEAT FUSED LOW PROFILE PREFORM THERMOPLASTIC (90 MILS).



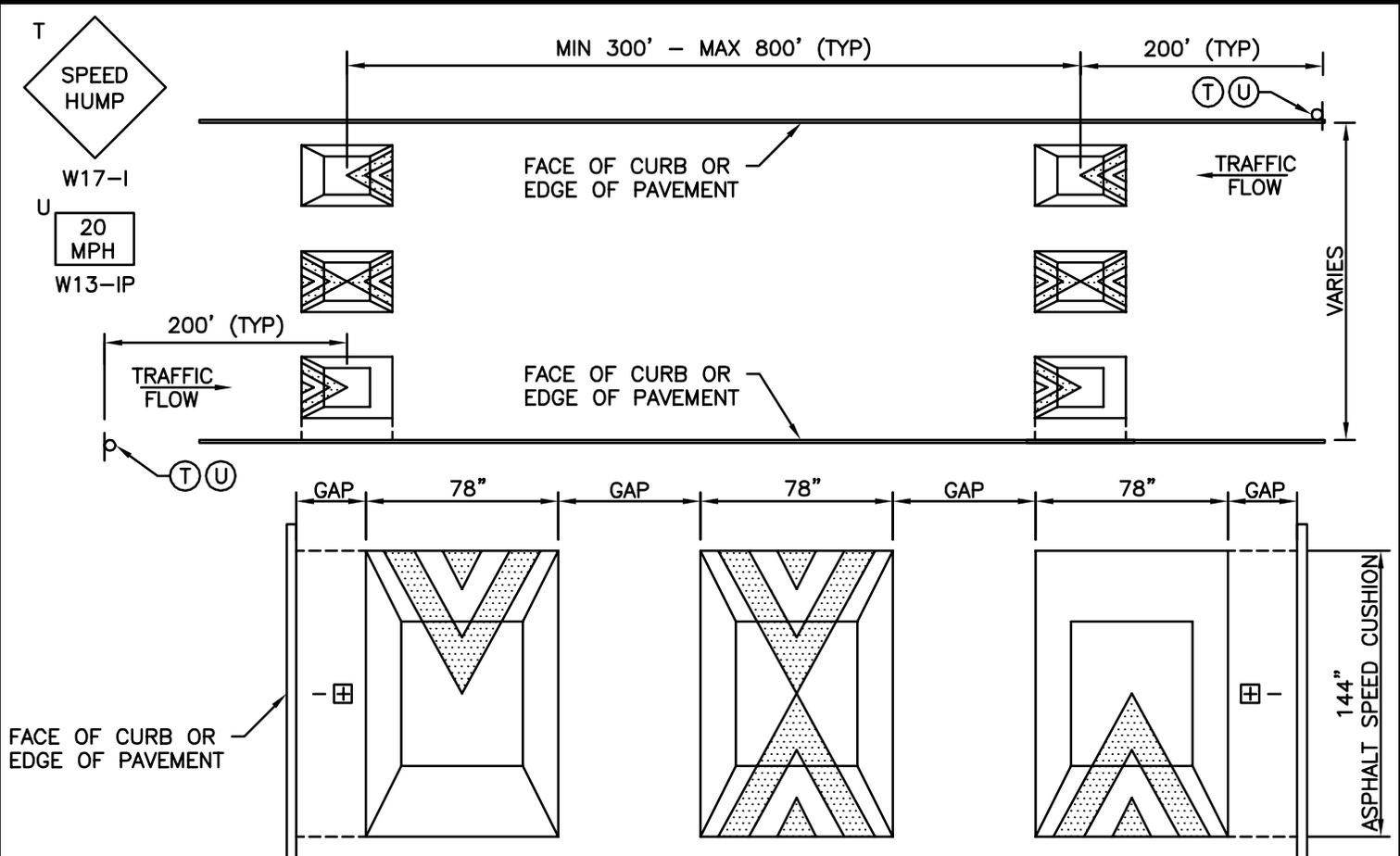
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

CROSSWALK DETAIL

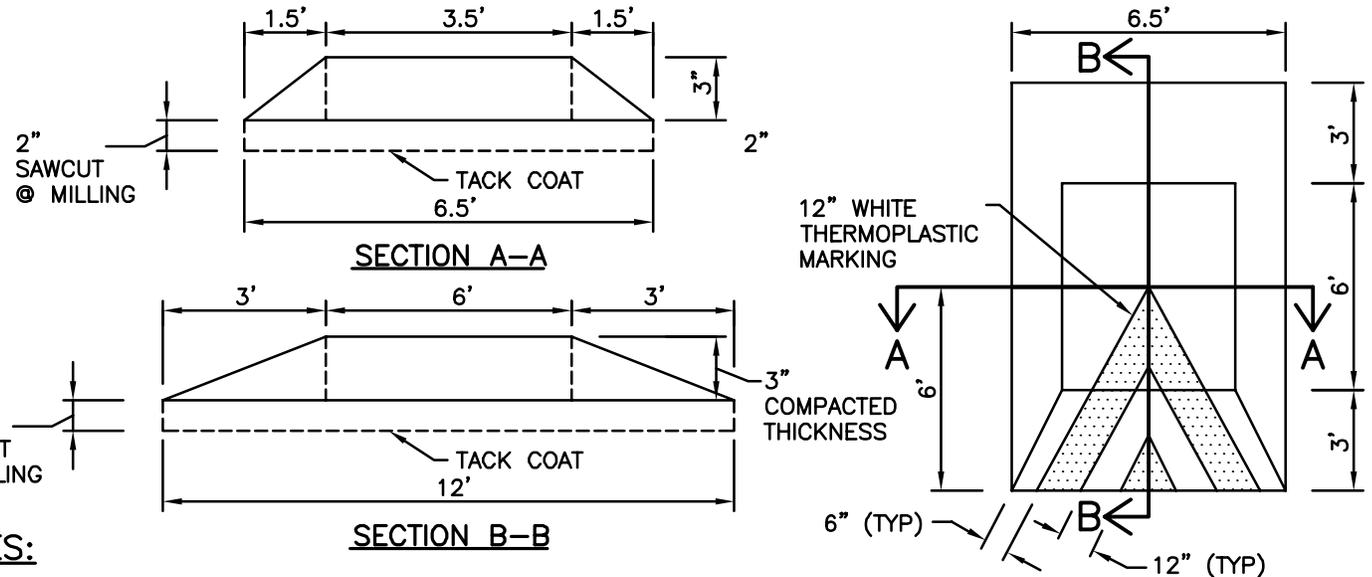
APPROVED BY: R. ENGLISH

REVISION DATE
JUNE 2019

STANDARD
DETAIL
TR-562

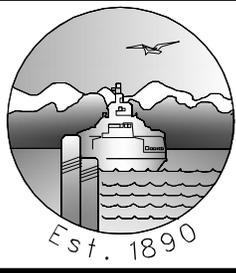


ASPHALT SPEED CUSHION - INSTALLATION DETAILS



NOTES:

1. NO PART OF SPEED CUSHION SHALL BE LOCATED IN FRONT OF DRIVEWAY APPROACH, WITH A MINIMUM OF 6 FEET FROM THE EDGE OF A DRIVEWAY.
2. ADVISORY SPEED CUSHION SIGNS SHALL BE PLACED 200' PRIOR TO ANY APPROACHING MOVEMENT, ALONG WITH ADVISORY SPEED LIMIT (TO BE DETERMINED BY CITY ENGINEER).



CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

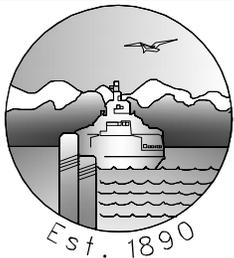
SPEED CUSHION

APPROVED BY: **R. ENGLISH**

REVISION DATE
FEBRUARY 2018

STANDARD
DETAIL
TR-563A

PAVEMENT WIDTH	NO. OF CUSHIONS	GAP (IN.)	CUSHION (IN.)	GAP (IN.)						
16	2	12	78	12	78	12				
18	2	20	78	20	78	20				
20	2	30	78	24	78	30				
22	2	36	78	36	78	36				
24	3	15	78	12	78	12	78	15		
26	3	20	78	19	78	19	78	20		
28	3	27	78	24	78	24	78	27		
30	3	31	78	32	78	32	78	31		
32	4	18	78	12	78	12	78	12	78	18
34	4	21	78	18	78	18	78	18	78	21
36	4	24	78	24	78	24	78	24	78	24
38	4	30	78	28	78	28	78	28	78	30
40	4	33	78	34	78	34	78	34	78	33



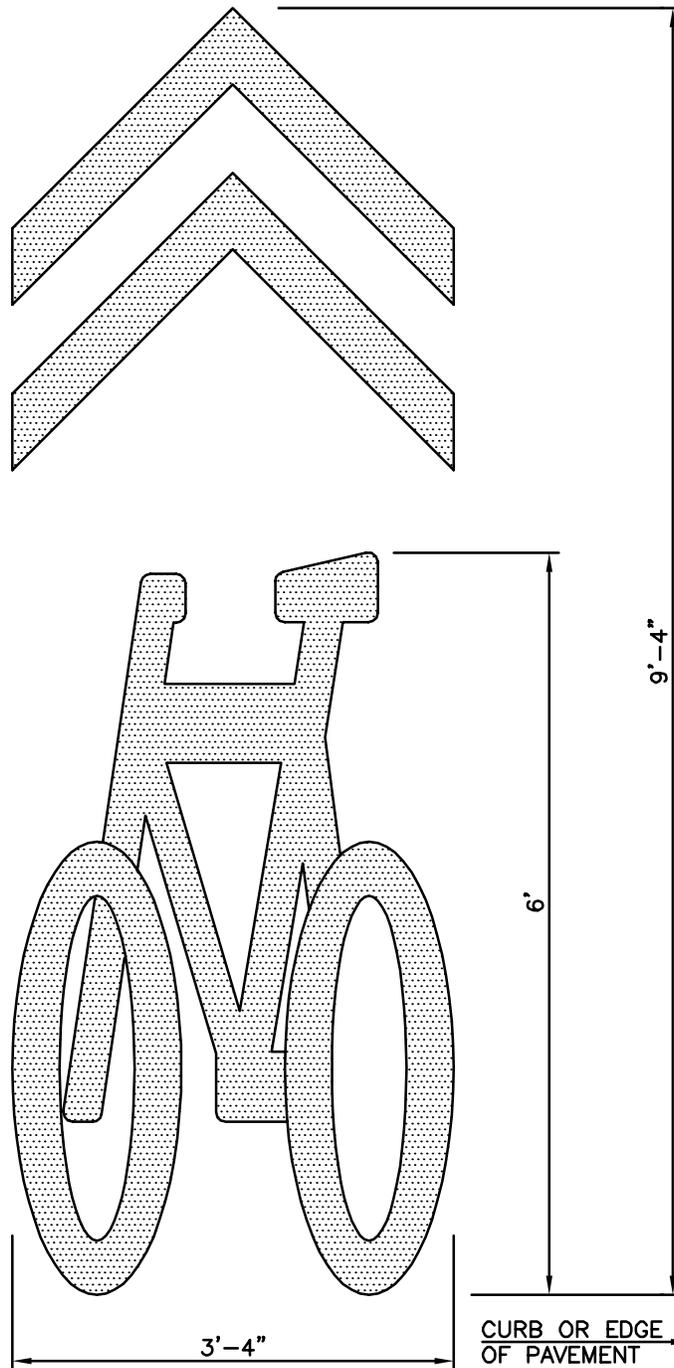
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**SPEED CUSHION
SIZING DETAIL**

REVISION DATE
FEBRUARY 2018

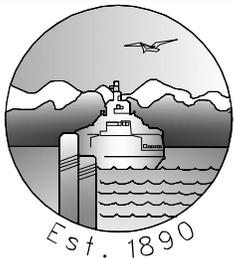
STANDARD
DETAIL
TR-563B

APPROVED BY: **R. ENGLISH**



NOTES:

1. MATERIAL SHALL BE HEAT FUSED LOW PROFILE PREFORMED THERMOPLASTIC (90 MIL).
2. ADJUSTMENTS TO DIMENSIONS SHALL BE APPROVED BY THE CITY ENGINEER.
3. IF USED IN A SHARED LANE WITH ON-STREET PARKING, SHARED LANE MARKINGS SHOULD BE PLACED SO THAT CENTER OF MARKING IS AT LEAST 11' FROM FACE OF CURB OR EDGE OF PAVEMENT IF NO CURB.
4. IF USED ON STREET WITHOUT ON STREET PARKING, CENTER OF MARKING SHOULD BE AT LEAST 4' FROM FACE OF CURB OR EDGE OF PAVEMENT IF NO CURB.



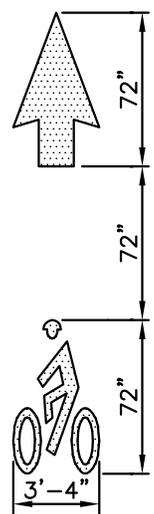
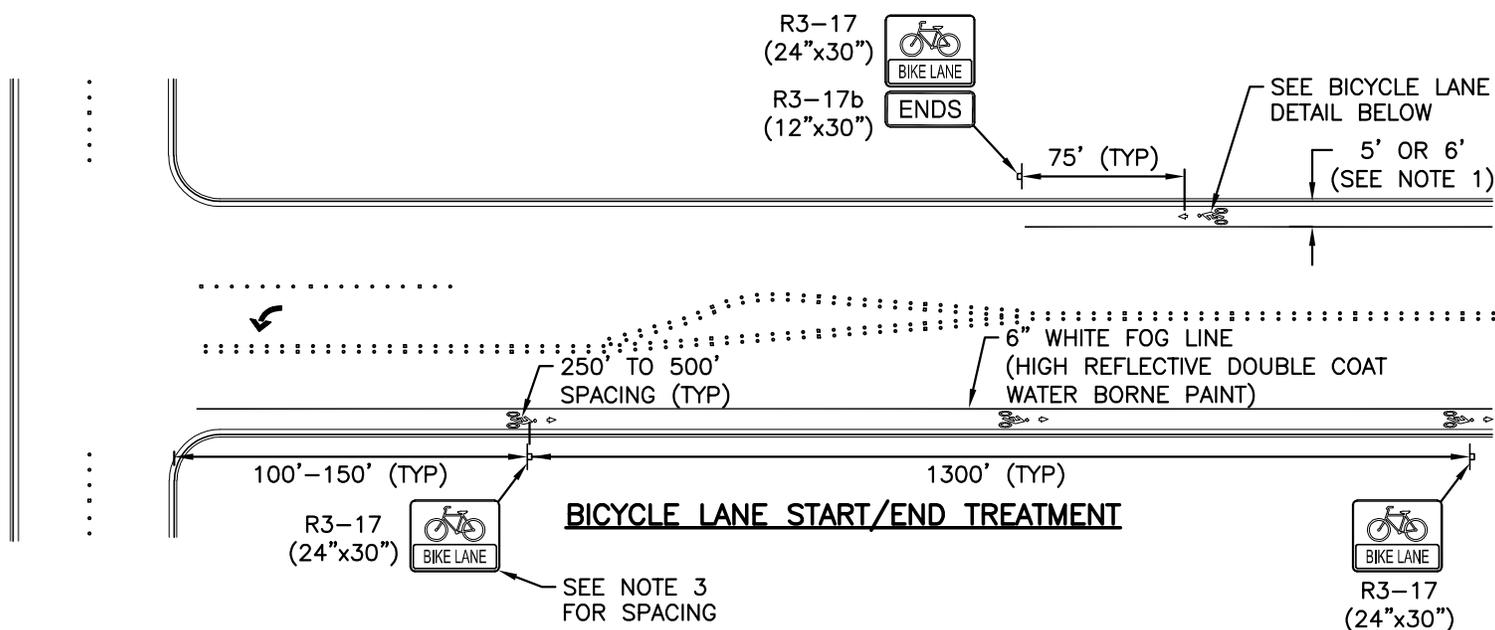
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

SHARED LANE MARKINGS

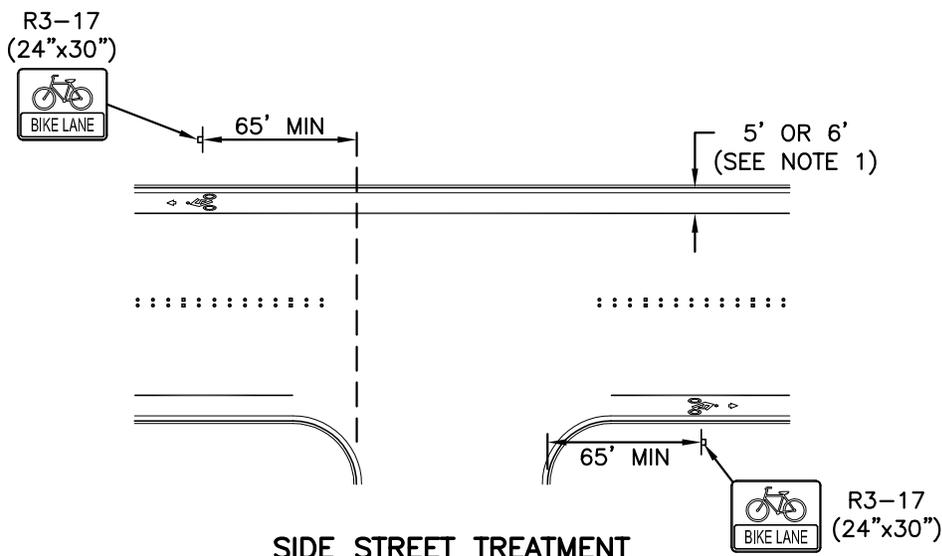
APPROVED BY: **R. ENGLISH**

REVISION DATE
JANUARY 2018

STANDARD
DETAIL
TR-564



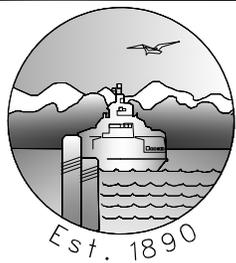
BICYCLE LANE DETAIL



SIDE STREET TREATMENT

NOTES:

1. BIKE LANE SYMBOLS SHALL BE HEAT FUSED LOW PROFILE PREFORM THERMOPLASTIC (90 MILS).
2. BIKE LANE WIDTH IS 5 FEET WITHOUT ON-STREET PARKING AND 6' WITH ON-STREET PARKING.
3. WHEN SIGN R3-17 IS USED, PAVEMENT MARKING SHALL BE INSTALLED ADJACENT TO R3-17.
4. R3-17 SIGN SHALL BE SPACED EVERY 1,300' (TYP), AND DOWNSTREAM OF PUBLIC SIDE STREETS.
5. CATCH BASINS SHALL HAVE VANED GRATES WHEN LOCATED WITHIN BICYCLE LANE.



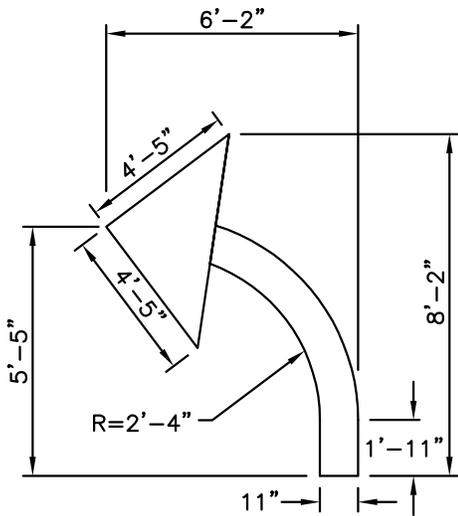
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**BICYCLE LANE
CHANNELIZATION**

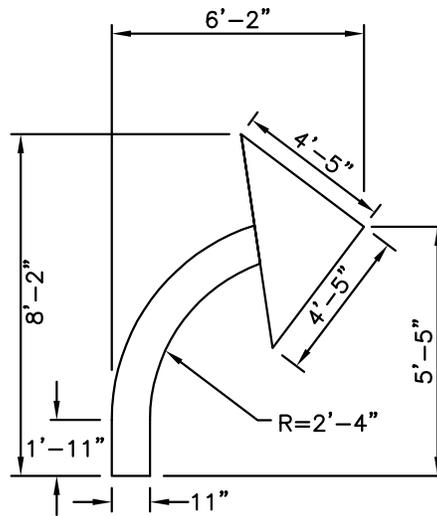
APPROVED BY: R. ENGLISH

REVISION DATE
SEPTEMBER 2019

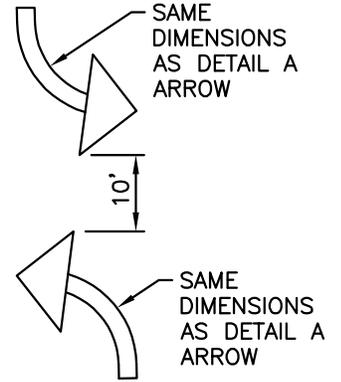
STANDARD
DETAIL
TR-565



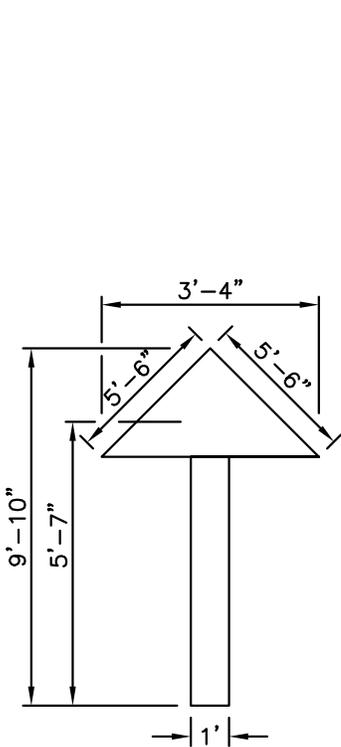
**DETAIL A
LEFT**



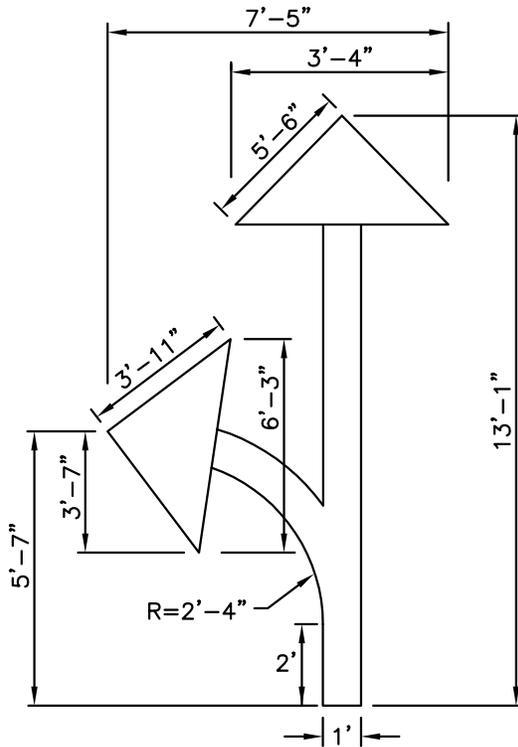
**DETAIL A
RIGHT**



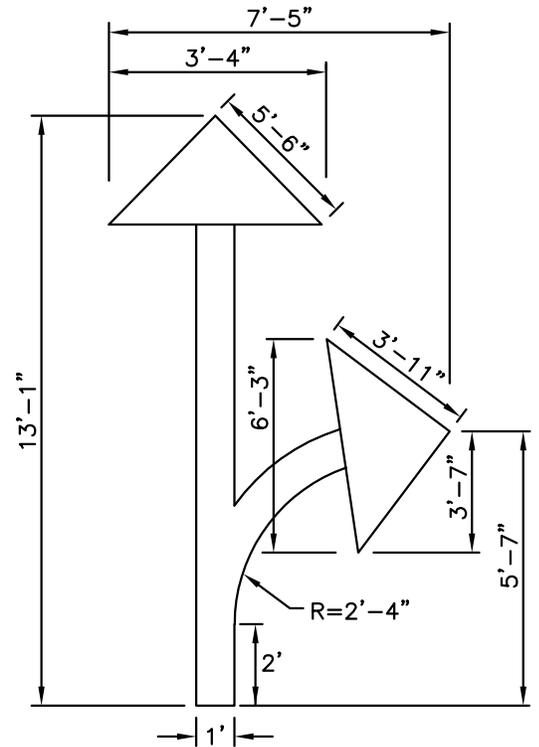
**DETAIL C
TWO WAY LEFT TURN**



**DETAIL D
STRAIGHT**



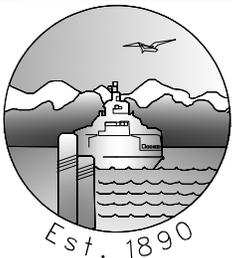
**DETAIL E
STRAIGHT/LEFT**



**DETAIL F
STRAIGHT/RIGHT**

NOTE:

1. MATERIAL SHALL BE HEAT FUSED LOW PROFILE PREFORMED THERMOPLASTIC, UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.



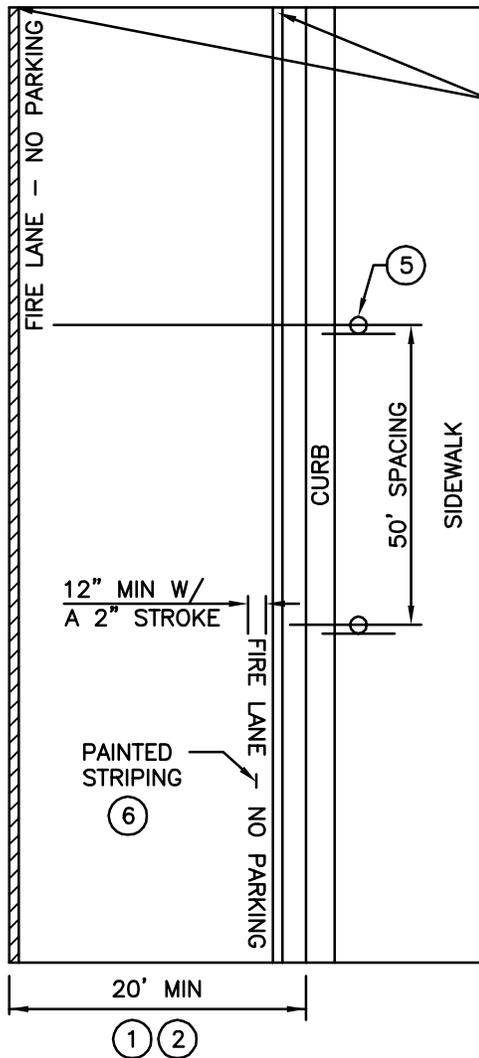
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**PAVEMENT ARROW
MARKINGS**

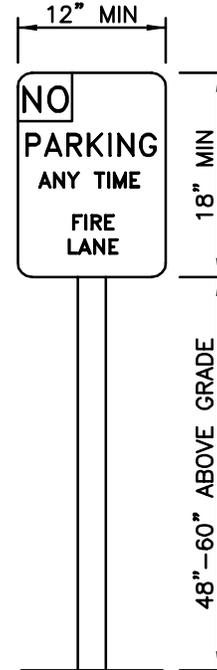
APPROVED BY: R. ENGLISH

REVISION DATE
OCTOBER 2018

STANDARD
DETAIL
TR-566



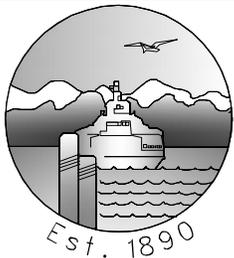
4" PERIMETER STRIPING
(TO BE 6" OFFSET FROM
CURB OR EDGE OF PAVEMENT)



* THE USE OF SIGNS REQUIRES
FIRE MARSHAL APPROVAL

NOTES:

- ① FIRE LANE MUST MEET ALL MINIMUM CITY OF EDMONDS DRIVEWAY/ACCESS STANDARDS AND SOUTH SNOHOMISH COUNTY REGIONAL FIRE AUTHORITY'S (SSCRFA) FIRE LANE STANDARD.
- ② FIRE LANE SHALL MAINTAIN AN UNOBSTRUCTED MINIMUM WIDTH OF 20' UNLESS APPROVED BY THE FIRE MARSHAL.
- ③ FIRE LANE TO BE ASPHALT OR APPROVED EQUIVALENT.
- ④ FIRE LANE SHALL MAINTAIN AN UNOBSTRUCTED MINIMUM VERTICAL CLEARANCE OF 13.5'.
- ⑤ FIRE LANE SHALL NOT HAVE A GRADE EXCEEDING 12% UNLESS APPROVED BY SSCRFA.
- ⑥ PAINTED STRIPING SHALL BE YELLOW IN COLOR.
- ⑦ REFER ALSO TO CITY OF EDMONDS AND SSCRFA FIRE LANE STANDARD FOR CIRCULATION AND TURN-AROUNDS.



CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**FIRE LANE SPECIFICATIONS
(MULTI-FAMILY AND
COMMERCIAL)**

APPROVED BY: **R. ENGLISH**

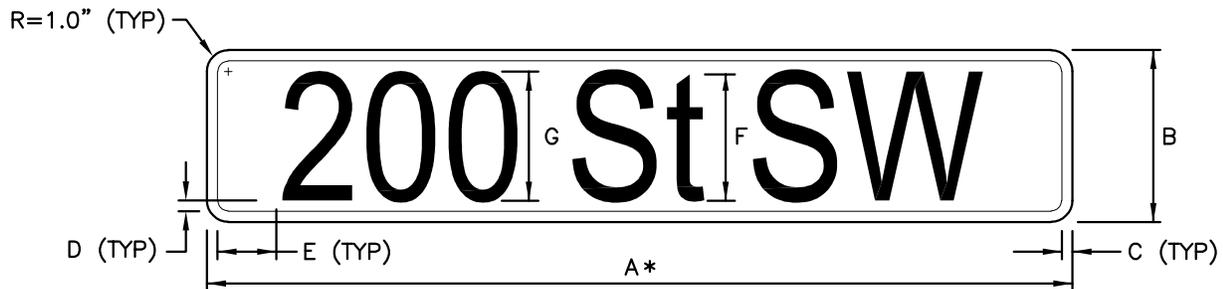
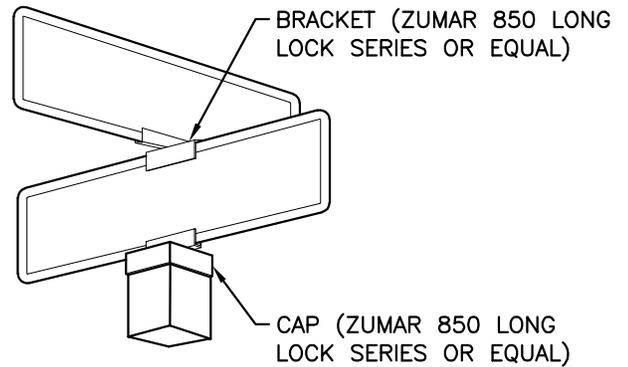
REVISION DATE
JANUARY 2018

STANDARD
DETAIL
TR-567



NOTES:

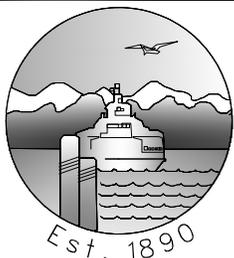
- * THE ADDRESS NUMBERS SHOULD BE 3" HIGH REFLECTIVE WHITE VINYL SET ON A ENGINEERING GREEN VINYL BACKGROUND.
- * ADDRESS SIGN REQUIRED WHEN ADDRESS CAN NOT BE SEEN FROM THE STREET AND PLACED ON 4"x4" TREATED WOOD POST. (SIGN TO BE PRODUCED BY CITY)
- * HEIGHT TO BOTTOM OF SIGN SHALL BE 30".



SIGN TYPE	A	B	C	D	E	F	G
POST MOUNTED 25 & 30 MPH	30" MIN	6"	.5"	1.5"	1.5" MIN	4" C	6" C
MOUNTED 35 MPH	VAR	9"	.5"	2.5"	4" MIN	4.5" C	6" C
MOUNTED STATE ROUTE	VAR	9"	.5"	2.5"	5.3" MIN	6" C	8" C
OVERHEAD(SIGNAL MAST ARMS)	VAR	16"	.5"	3.5"	6" MIN	9" C	12" C

NOTES:

1. MATERIAL – .080 FLAT ALUMINUM ROUNDED CORNERS WITH HIGH INTENSITY/PRISMATIC DIAMOND GRADE SHEETING.
 2. COLOR – GREEN BACKGROUND WITH WHITE LETTERS, NUMBERS AND BORDER ON BOTH SIDES OF THE SIGN.
 3. TEXT SHALL BE FHWA D SERIES FONT AND LAID OUT AS PER THE STANDARD HIGHWAY SIGNS AND MARKING (SHSM) BOOK.
 3. ALL LETTERING SHALL BE COMPOSED OF UPPER AND LOWER CASE LETTERS.
 4. ALL MOUNTED SIGNS FACING TRAFFIC ON 35 MPH STREETS SHALL HAVE THE NAME OF THE STREET IN 6" LETTERING.
 5. ALL MOUNTED SIGNS FACING TRAFFIC ON STATE ROUTE SHALL HAVE THE NAME OF THE STREET IN 6" LETTERING INCLUDING, TYPE ABBREVIATED AS (ST, AVE, DR, PL, WAY, CIR, ETC) AND GEOGRAPHIC DELINEATORS (W AND SW).
 6. ALL OVERHEAD MOUNTED (TRAFFIC SIGNAL MAST ARMS) SIGNS SHALL HAVE THE NAME OF THE STREET IN 12" LETTERING.
- * VARIABLE LENGTHS REQUIRED FOR NAMED STREETS.



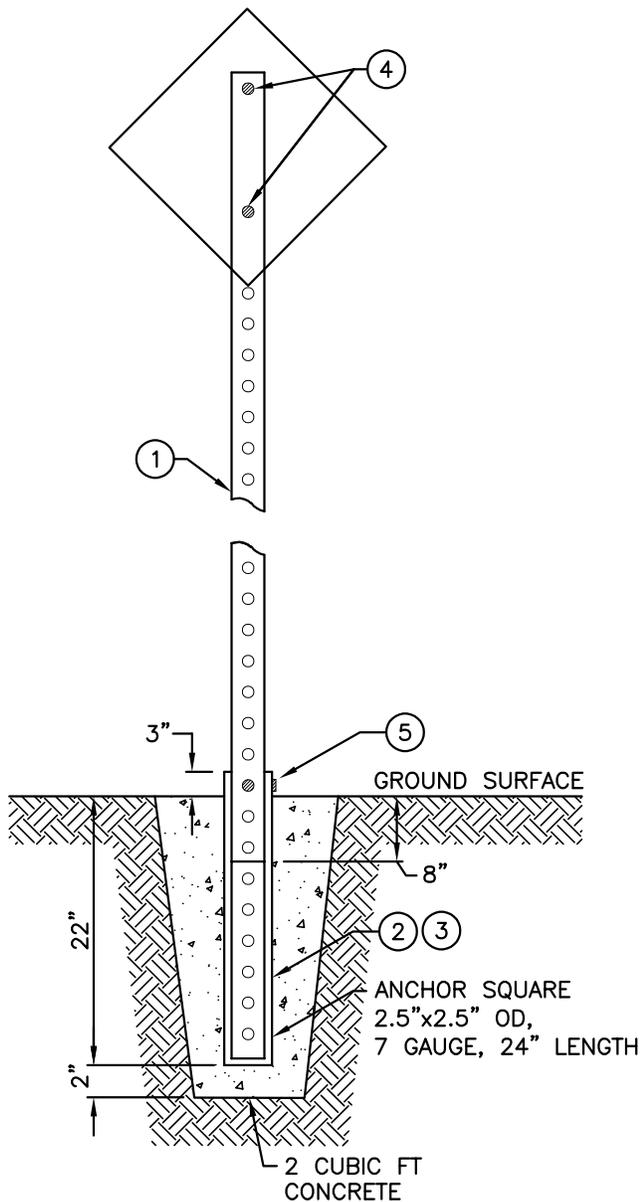
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**STREET NAME SIGN AND
FIRE-AID ADDRESS AND
POST MOUNTING DETAIL**

APPROVED BY: R. ENGLISH

REVISION DATE
OCTOBER 2018

STANDARD
DETAIL
TR-570



SQUARE METAL POST

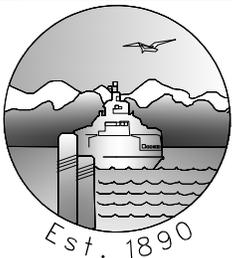
NOTES:

SIGN POST:

- ① SIGN POST SHALL BE 2"x2" SQUARE STEEL POSTS, MINIMUM 14 GAUGE, WITH 7/16" DIE-PUNCHED KNOCKOUTS ON 1" CENTERS FULL LENGTH FOUR SIDES COMPLYING W/ ASTM SPEC A653 AND BE HOT DIPPED GALVANIZED.
- ② ANCHOR SHALL BE 2.25"x2.25" OD SQUARE STEEL, MINIMUM 14 GAUGE WITH 7/16" DIE PUNCHED KNOCKOUTS ON 1" CENTER COMPLYING WITH ASTM SPEC. A653 AND BE HOT DIPPED GALVANIZED. ANCHOR SHALL HAVE MINIMUM OF 3 HOLES ABOVE GRADE.
- ③ ANCHOR SHALL HAVE TAPE INSTALLED ON BOTTOM AS WELL AS ALL SIDES TO KEEP INSIDE CLEAR OF ALL MATERIAL.
- ④ INSTALL TWO DRIVE RIVETS
ZUMAR INDUSTRIES #TL3806 OR EQUIVALENT
- ⑤ INSTALL TWO CORNER BOLTS AND NUTS
ZUMAR INDUSTRIES #TL070 OR EQUIVALENT
- ⑥ 4"x4" WOODEN PRESSURE TREATED POST MAY BE USED WHEN POST IS MORE THAN 10' FROM EDGE OF TRAVEL LANE (OR APPROVED BY ENGINEER).

SIGN INSTALLATION:

SEE TRAFFIC STREET SIGN DETAIL TR-575 FOR GUIDANCE ON SIGN INSTALLATIONS.



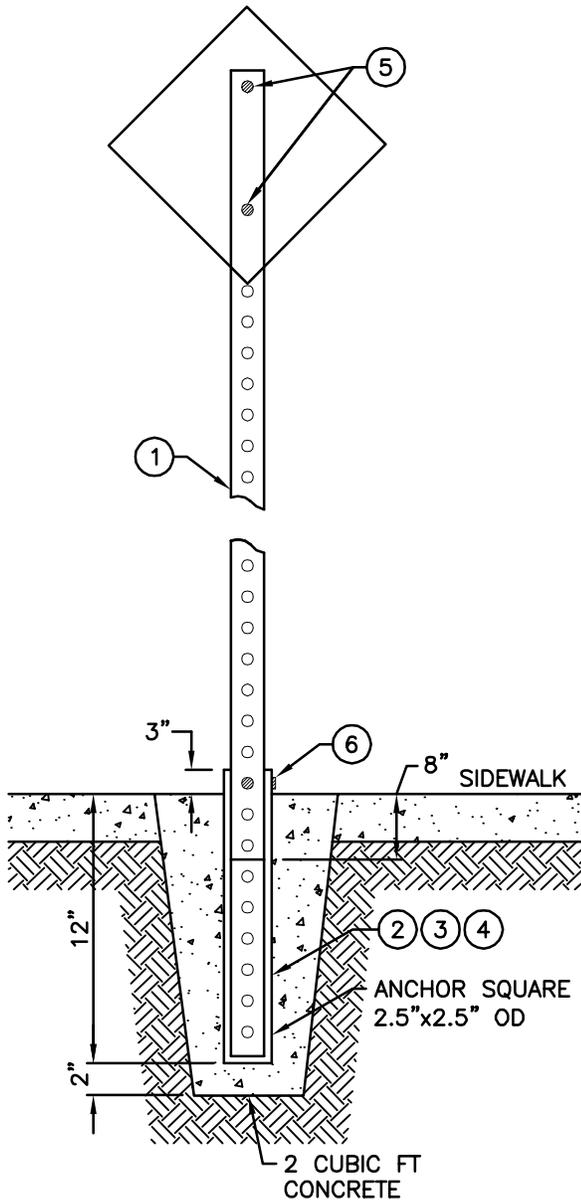
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**SIGN POST INSTALLATION
IN NON-SIDEWALK AREA**

APPROVED BY: R. ENGLISH

REVISION DATE
OCTOBER 2018

STANDARD
DETAIL
TR-571



SQUARE METAL POST

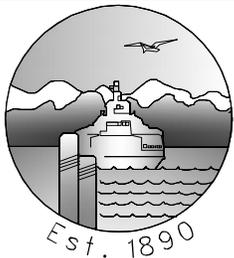
NOTES:

SIGN POST:

- ① SIGN POST SHALL BE 2"x2" SQUARE STEEL POSTS, MINIMUM 14 GAUGE, WITH 7/16" DIE-PUNCHED KNOCKOUTS ON 1" CENTERS FULL LENGTH FOUR SIDES COMPLYING W/ ASTM SPEC A653 AND BE HOT DIPPED GALVANIZED.
- ② ANCHOR SHALL BE 2.25"x2.25" OD SQUARE STEEL, MINIMUM 14 GAUGE WITH 7/16" DIE PUNCHED KNOCKOUTS ON 1" CENTER COMPLYING WITH ASTM SPEC. A653 AND BE HOT DIPPED GALVANIZED. ANCHOR SHALL HAVE MINIMUM OF 3 HOLES ABOVE GRADE.
- ③ ANCHOR SHALL HAVE TAPE INSTALLED ON BOTTOM AS WELL AS ALL SIDES TO KEEP INSIDE CLEAR OF ALL MATERIAL.
- ④ FOR IN-SIDEWALK INSTALLATIONS, CORE 4" DIA HOLE. ANCHOR LENGTH SHALL BE 12".
- ⑤ INSTALL TWO DRIVE RIVETS
ZUMAR INDUSTRIES #TL3806 OR EQUIVALENT
- ⑥ INSTALL TWO CORNER BOLTS AND NUTS
ZUMAR INDUSTRIES #TL070 OR EQUIVALENT

SIGN INSTALLATION:

SEE TRAFFIC STREET SIGN DETAIL TR-575 FOR GUIDANCE ON SIGN INSTALLATIONS.



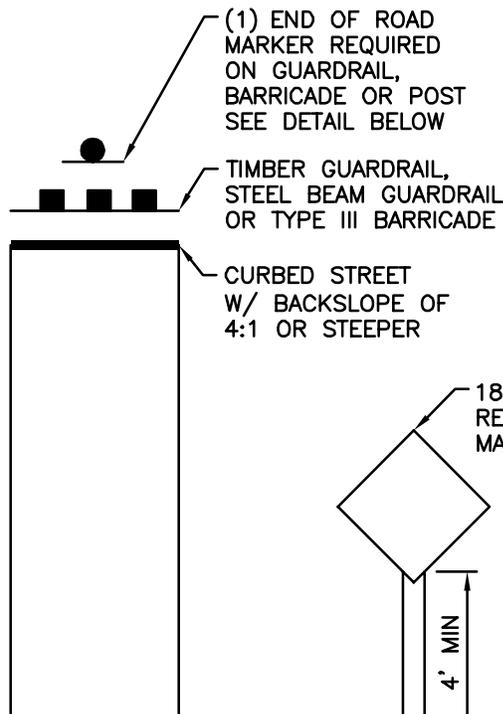
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**SIGN POST INSTALLATION
IN SIDEWALK AREA**

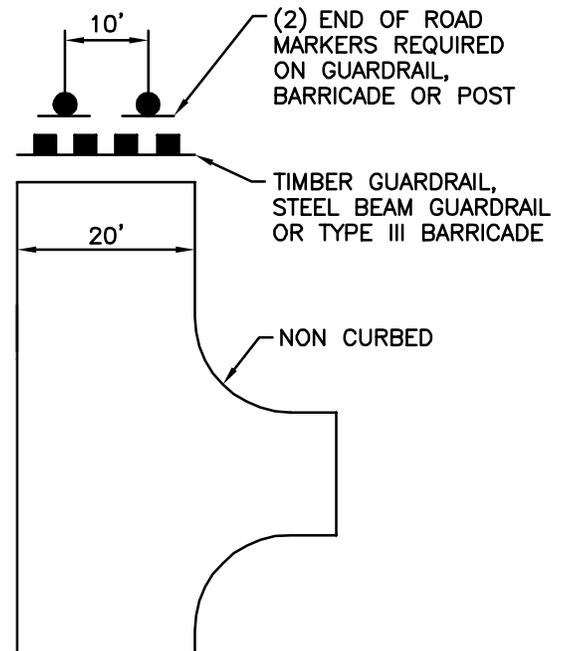
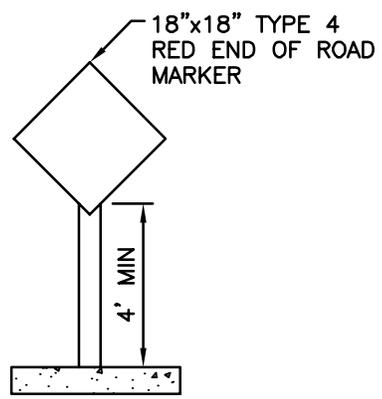
APPROVED BY: **R. ENGLISH**

REVISION DATE
FEBRUARY 2018

STANDARD
DETAIL
TR-572



ROADWAY WIDTH < 20'

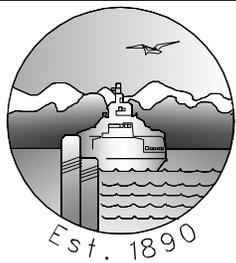


ROADWAY WIDTH ≥ 20'

NOTES:

REQUIREMENTS FOR PUBLIC STREETS:

1. CUL-DE-SAC STREETS ARE EXEMPT FROM THE BELOW STANDARDS.
2. END OF ROADWAY DELINEATION SHALL COMPLY WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
3. THE GUARDRAIL OR BARRICADE SHOULD EXTEND ENTIRE ROADWAY WIDTH.
4. INSTALL A SINGLE END OF ROAD TYPE 4 MARKER, AS DEFINED BY THE MUTCD, AT THE END OF A PAVED STREET THAT IS LESS THAN 20 FEET IN WIDTH.
5. INSTALL TWO END OF ROAD TYPE 4 MARKERS AT THE END OF PAVED STREETS WITH A WIDTH OF 20 FEET OR MORE.
6. THE MINIMUM MOUNTING HEIGHT OF AN END OF ROAD MARKER SHALL BE 4 FEET ABOVE THE GROUND. END OF THE ROAD MARKERS WHEN USED WITH TYPE III BARRICADES OR GUARDRAIL, SHALL BE MOUNTED DIRECTLY TO THE BARRICADE OR GUARDRAIL.



CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**END OF ROADWAY
DELINEATION**

APPROVED BY: **R. ENGLISH**

REVISION DATE
JANUARY 2018

STANDARD
DETAIL
TR-573

○

CITY OF EDMONDS

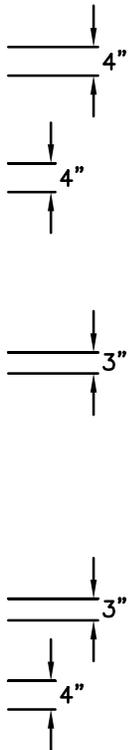
PROJECT TITLE

LOCATION: _____ FROM _____ TO _____

SCHEDULE

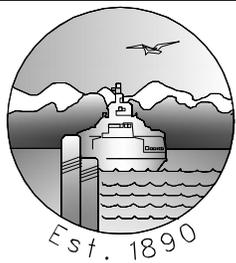
*CONTACT: PROJECT MANAGER NAME
AT 425-XXX-XXXX*

○



NOTES:

1. USE BLACK LETTERS ON CONSTRUCTION ORANGE BACKGROUND.
2. SIGN TO BE 36"x36" MINIMUM, PLACED IN LOCATIONS APPROVED BY CITY ENGINEER.



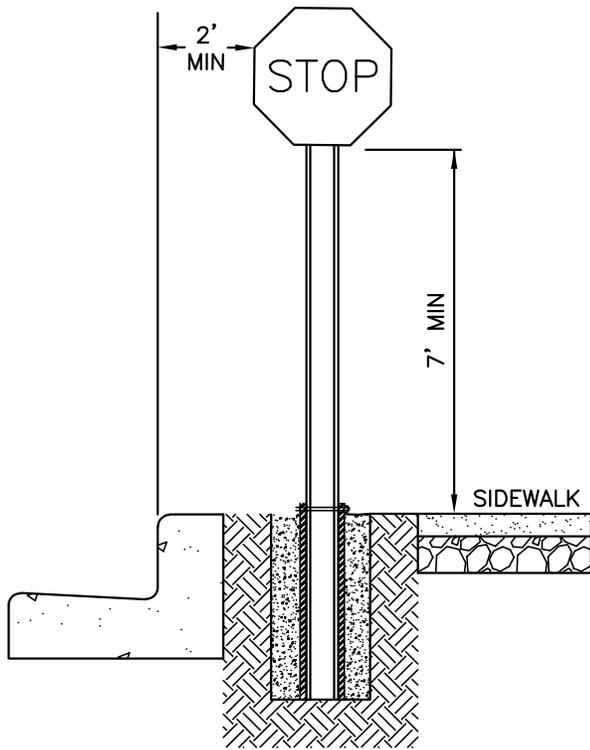
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**CONSTRUCTION NOTICE
SIGNAGE
(CAPITAL IMPROVEMENTS)**

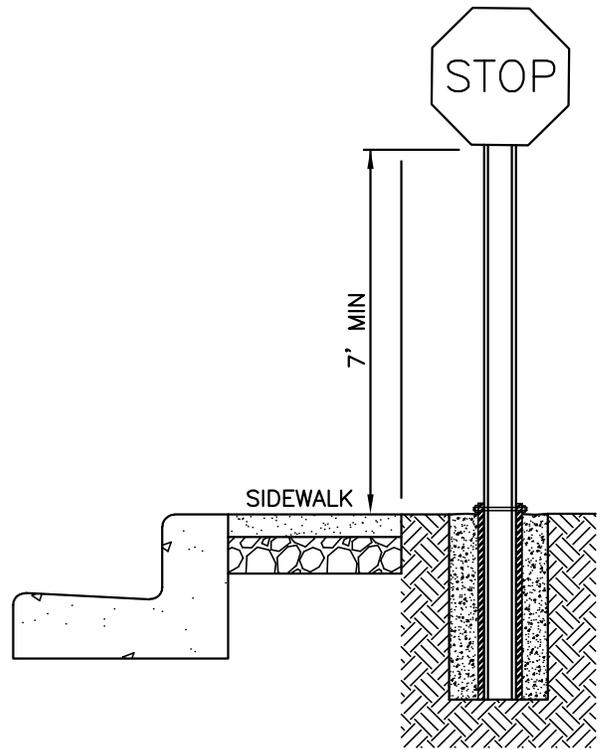
APPROVED BY: **R. ENGLISH**

REVISION DATE
JANUARY 2018

STANDARD
DETAIL
TR-574



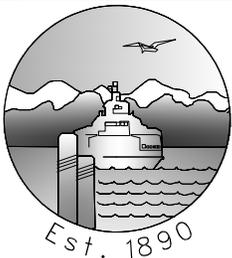
INSTALLATION BETWEEN CURB AND SIDEWALK



INSTALLATION BEHIND SIDEWALK

NOTES:

1. SIGN HEIGHT SHALL BE 7' FROM THE BOTTOM OF SIGN TO STREET OR SIDEWALK. EXCEPTIONS ONLY AS SPECIFICALLY STATED ON PLANS OR AS APPROVED BY ENGINEER.
2. THE SIGN SHALL BE AT LEAST 2' BEHIND THE ROADWAY.
3. ALL SIGNS SHALL MEET MUTCD STANDARDS, INCLUDING RETRO-REFLECTIVITY.



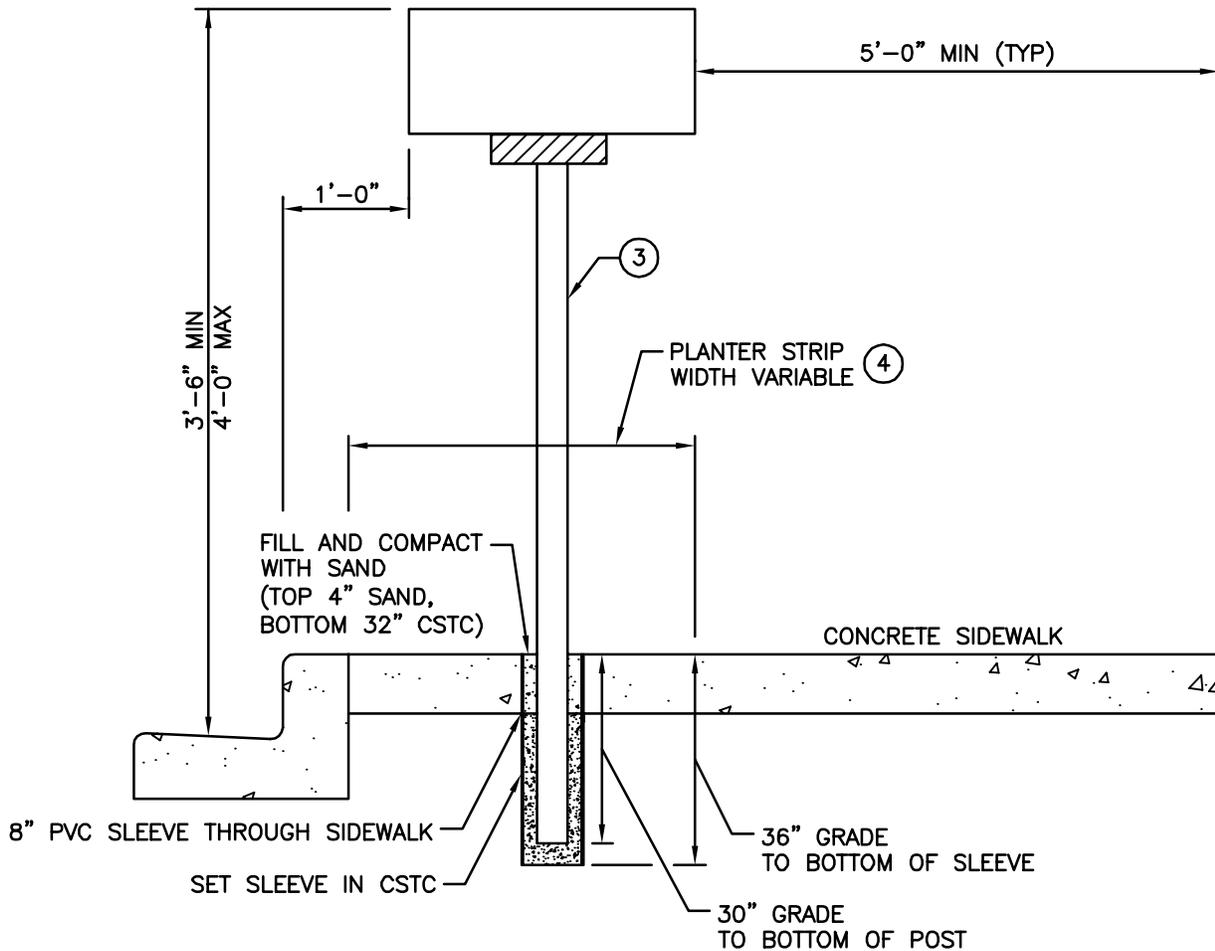
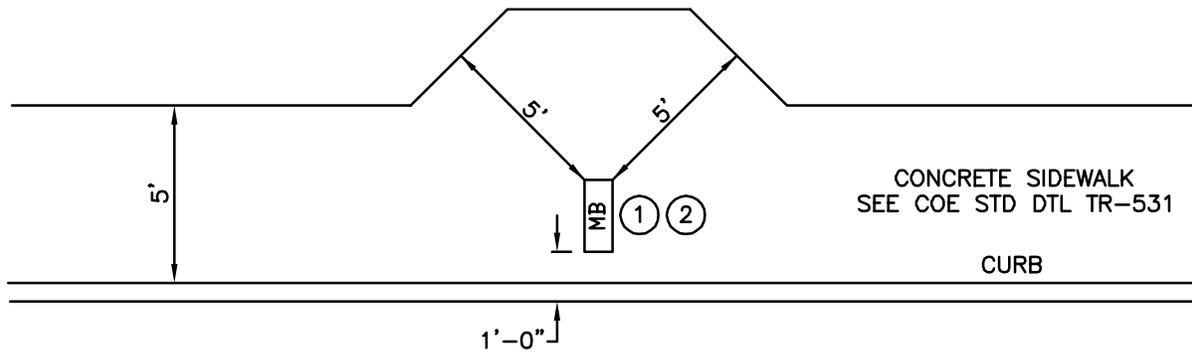
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

TRAFFIC STREET SIGNS

APPROVED BY: R. ENGLISH

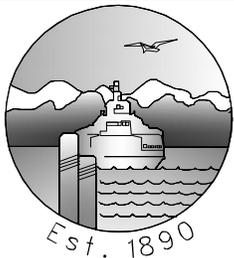
REVISION DATE
JUNE 2019

STANDARD
DETAIL
TR-575



NOTES:

- ① LOCATION OF MAILBOX WITHIN THE RIGHT-OF-WAY SHALL BE APPROVED BY THE CITY.
- ② MAILBOXES AND LOCK BOXES SHALL COMPLY WITH USPS REGULATIONS.
- ③ POST GIVING FIRM SUPPORT BUT NOT TO EXCEED 4x4 WOODEN POST (TREATED).
- ④ FOR MAILBOXES PLACED WITHIN PLANTER STRIP, FOLLOW COE STD DTL TR-582.



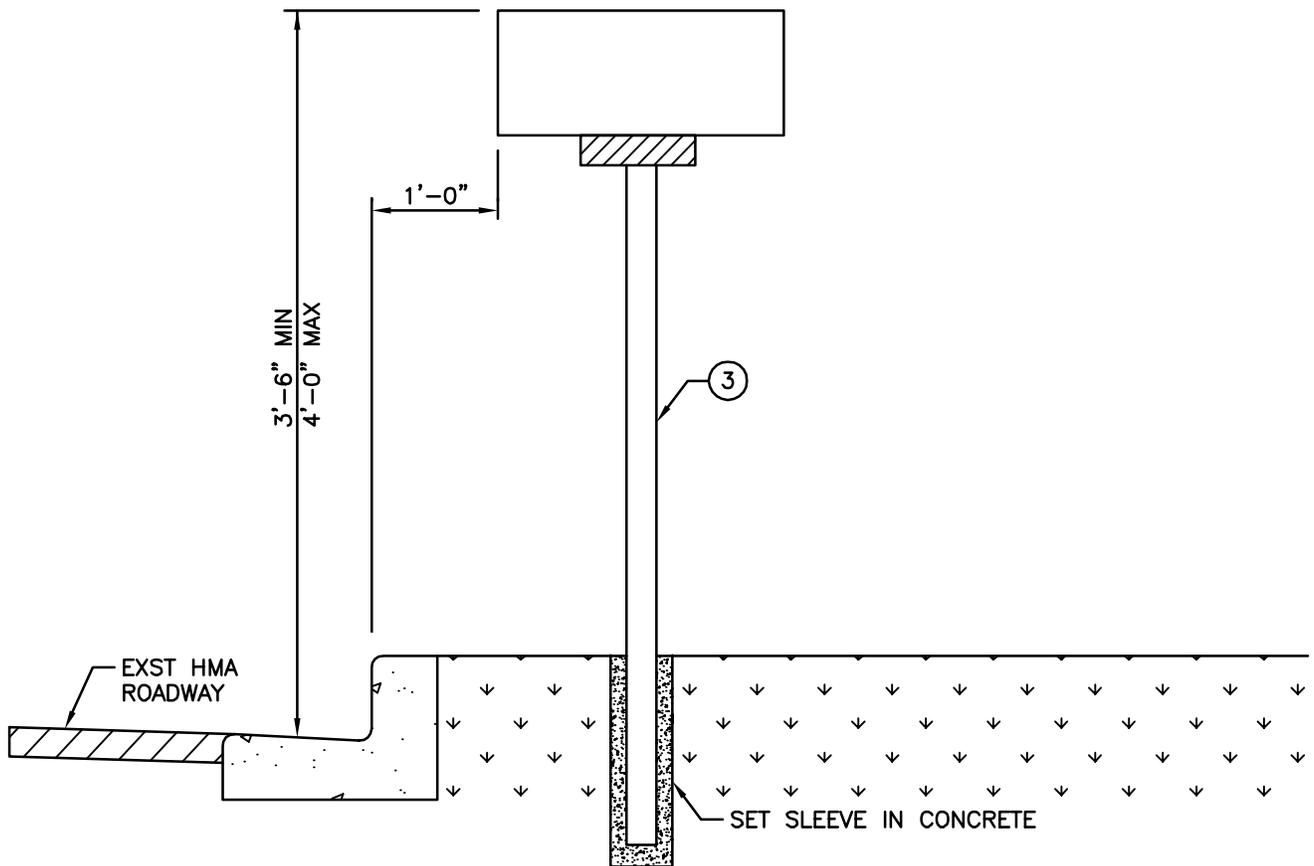
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**MAILBOX
DETAIL 1**

APPROVED BY: **R. ENGLISH**

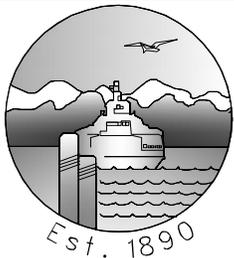
REVISION DATE
JANUARY 2018

STANDARD
DETAIL
TR-580



NOTES:

- ① LOCATION OF MAILBOX WITHIN THE RIGHT-OF-WAY SHALL BE APPROVED BY THE CITY.
- ② MAILBOXES AND LOCK BOXES SHALL COMPLY WITH USPS REGULATIONS.
- ③ POST GIVING FIRM SUPPORT BUT NOT TO EXCEED 4x4 WOODEN POST (TREATED).



CITY OF EDMONDS
 PUBLIC WORKS
 DEPARTMENT

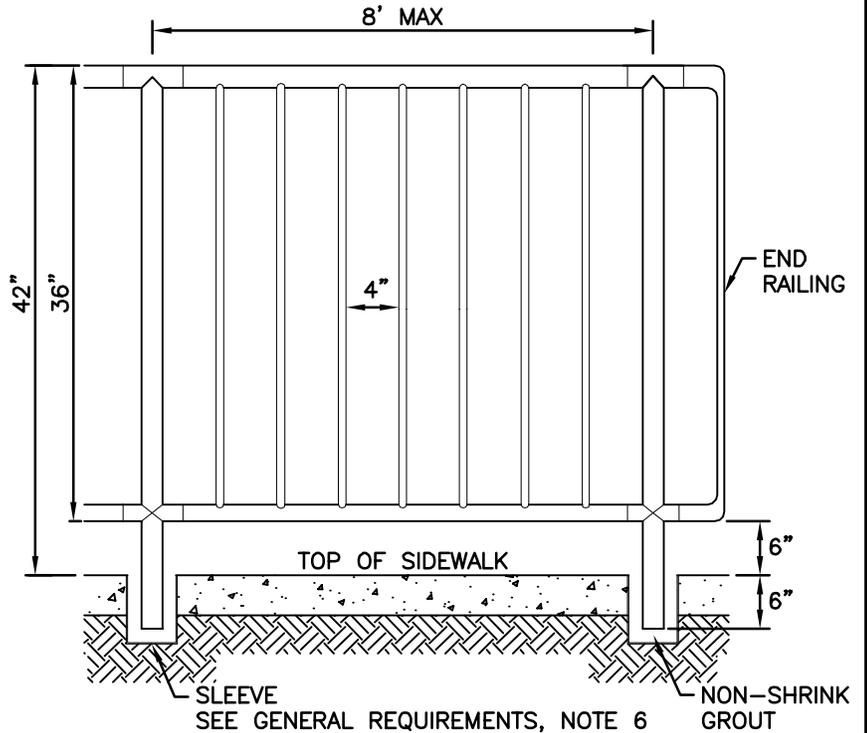
**MAILBOX
 DETAIL 2**

APPROVED BY: **R. ENGLISH**

REVISION DATE
JANUARY 2018

STANDARD
 DETAIL
TR-582

MATERIAL DIMENSIONS			
PANEL HEIGHT	TOP RAIL/ POST	BOTTOM RAIL	BALUSTER
36"	1-1/2" NOM.	1-1/2" NOM.	7/8"Ø BAR



NOTES:

MATERIAL REQUIREMENTS:

1. GALVANIZED STEEL RAILING SHALL BE USED.

GENERAL REQUIREMENTS:

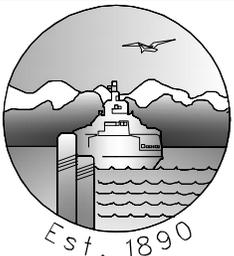
1. CUTTING SHALL BE DONE BY SAWING OR MILLING AND ALL CUTS SHALL BE TRUE AND SMOOTH. FLAME CUTTING WILL NOT BE PERMITTED.
2. ALLOW FOR EXPANSION AT APPROXIMATELY EVERY FOURTH POST.
3. SLEEVES SHALL BE SCHEDULE 40 PVC AND HAVE AN INSIDE DIAMETER 2 INCHES LARGER THAN THE POST OUTSIDE DIAMETER. IF RAILING IS TO BE INSTALLED IN EXISTING SIDEWALK, HOLES SHALL BE CORE DRILLED 2 INCHES LARGER THAN THE OUTSIDE DIAMETER OF THE POST, AND NO LESS THAN 6" FROM THE EDGE OF CONCRETE.
4. AVOID PLACING SAFETY RAIL IMPACTING SIGHT LINES. SEE COE STD DTL TR-590.

ALUMINUM RAILING REQUIREMENTS:

1. RAILING SHALL BE CV PIPE RAIL OR APPROVED EQUIVALENT. INSTALLATION PER MANUFACTURER'S RECOMMENDATIONS. BALUSTERS SHALL BE SOLID ALUMINUM TACK WELDED IN PLACE.
2. ALL ALUMINUM PARTS SHALL BE GIVEN A CLEAR ANODIC COATING AT LEAST 0.0006 INCH THICK AND BE HOT WATER SEALED AND SHALL HAVE A UNIFORM FINISH.
3. PIPE RAILING AND PIPE RAILING SPLICES MAY BE HEATED TO NOT MORE THAN 400°F FOR A PERIOD NOT TO EXCEED 30 MINUTES TO FACILITATE FORMING OR BENDING.
4. WELDING OF ALUMINUM SHALL BE IN ACCORDANCE WITH THE LATEST AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS.
5. RAILS, POSTS AND FORMED ELBOWS SHALL BE A.S.T.M. B-241 OR B-429 ALLOW 6063-T6 SCHEDULE 40 (STD PIPE). BRACKETS, END CAPS AND OTHER FITTINGS SHALL BE A.S.T.M. 6063-T5. SPLICES AND REINFORCING SLEEVES SHALL BE DRAWN ALUMINUM TUBING 6063-T832. SLEEVE I.D. SHALL BE 1" GREATER THAN POST O.D.

STEEL RAILING REQUIREMENTS:

1. POST AND RAIL MATERIAL SHALL BE SCHEDULE 40 STEEL PIPE CONFORMING TO ASTM A 53, GRADE B. BALUSTERS SHALL BE SOLID STEEL BARS CONFORMING TO AASHTO M 183.
2. SPOT WELDING IS NOT ALLOWED. ALL WELDS SHALL ENCOMPASS THE ENTIRE JOINT.
3. SAFETY RAILING WILL BE HOT DIPPED GALVANIZED AFTER FABRICATION.
4. ANY FIELD CUTTING OR WELDING AREAS SHALL BE GROUND SMOOTH AND COATED WITH AT LEAST 2 COATS OF COLD GALVANIZED PAINT.



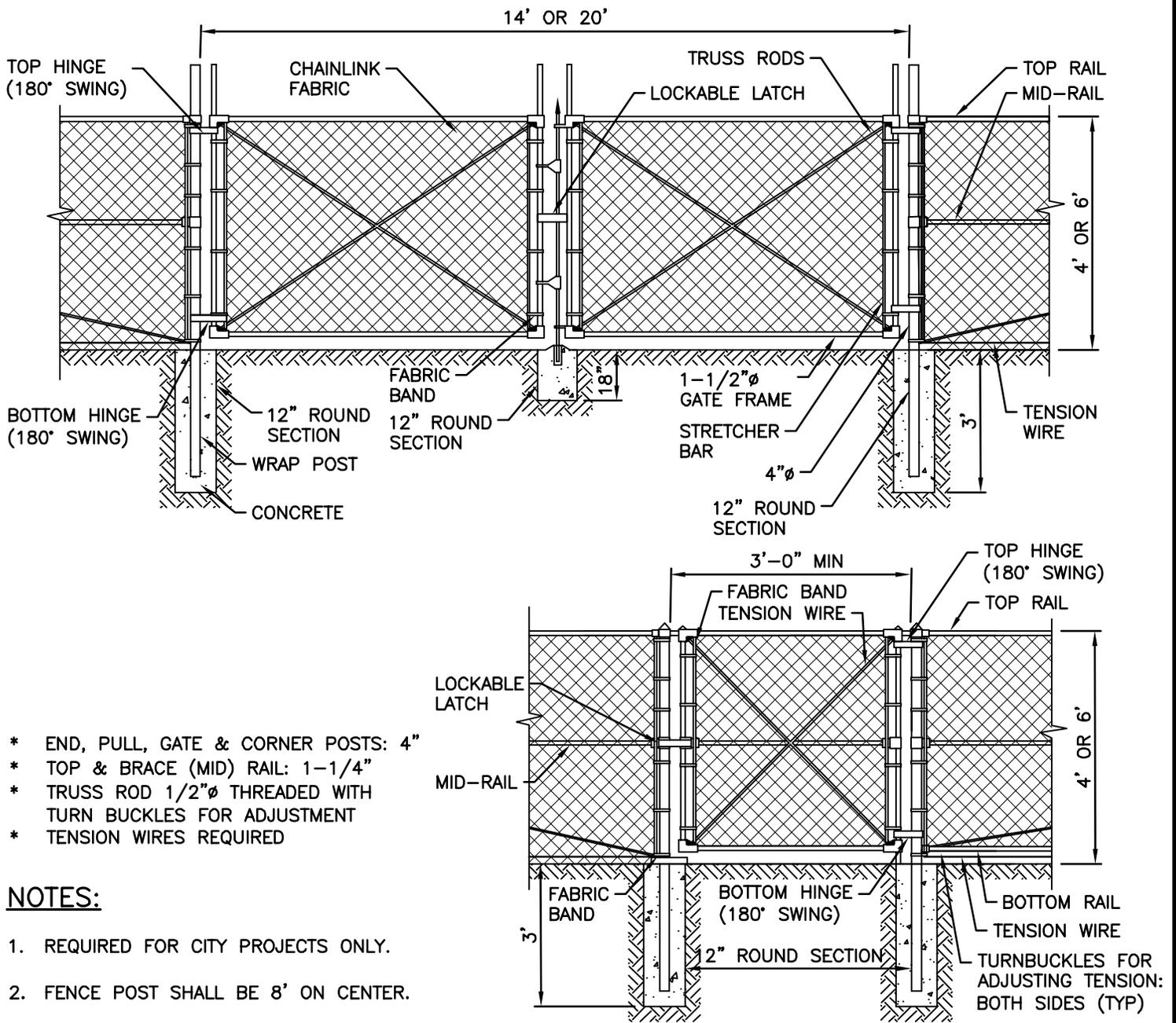
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

METAL SAFETY RAILING

APPROVED BY: R. ENGLISH

REVISION DATE
JANUARY 2018

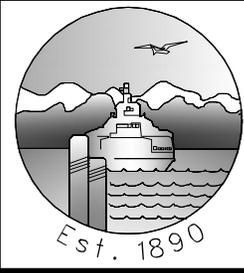
STANDARD
DETAIL
TR-583



- * END, PULL, GATE & CORNER POSTS: 4"
- * TOP & BRACE (MID) RAIL: 1-1/4"
- * TRUSS ROD 1/2"Ø THREADED WITH TURN BUCKLES FOR ADJUSTMENT
- * TENSION WIRES REQUIRED

NOTES:

1. REQUIRED FOR CITY PROJECTS ONLY.
2. FENCE POST SHALL BE 8' ON CENTER.
3. BARBED WIRE MAY BE ADDED AS DIRECTED BY THE ENGINEER.
4. FENCE FABRIC SHALL BE SECURED TO GATE FRAMES WITH KNUCKLED SELVAGE ALONG TOP EDGE FOR CHAIN LINK FENCE INSTALLATIONS.
5. MINIMUM POST LENGTH: 9'-0" OR 7'-0" (WITH 3'-0" IN CONCRETE).
6. ALL COMPONENTS OF FENCE SHALL BE VINYL COATED-COLOR BLACK.
7. PLASTIC PRIVACY SLATS TO BE INSTALLED, WHEN APPROVED BY THE ENGINEER, & EACH SLAT WILL BE SECURED TO FENCE BY DRILLING HOLE IN BOTTOM OF SLAT & SECURING THEM TO FENCE USING ALUMINUM TWIST WIRE.
8. FOR CITY APPLICATIONS, 3 STRAND BARBED WIRE SHALL BE INSTALLED ATOP THE FENCE.



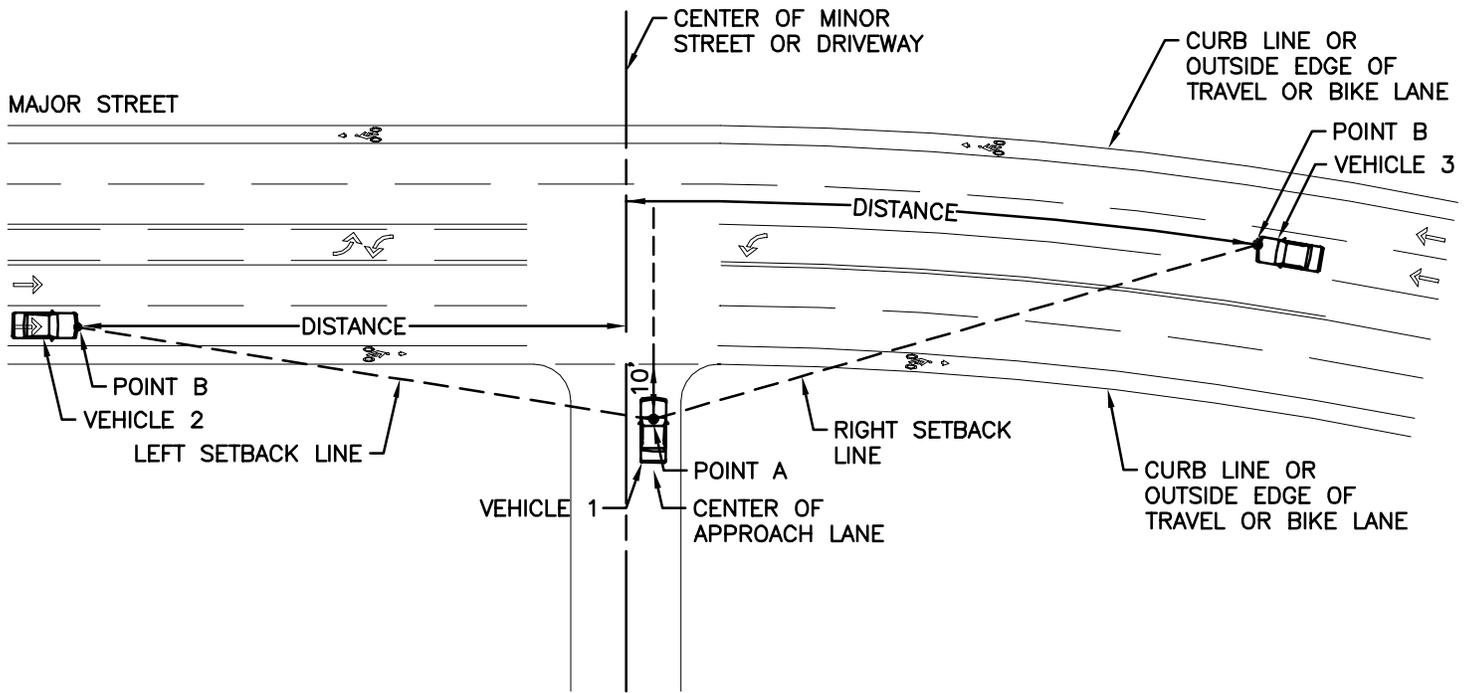
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

CHAIN LINK FENCE

APPROVED BY: **R. ENGLISH**

REVISION DATE
JANUARY 2018

STANDARD
DETAIL
TR-584



**MINOR STREET, PRIVATE ROAD,
COMMERCIAL, RESIDENTIAL DRIVEWAY
AND SIGNALIZED INTERSECTION**

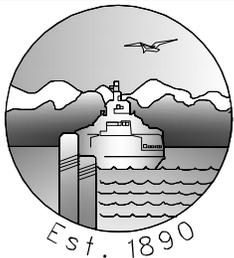
NOTES:

1. DISTANCE MEASURED FROM AN EYE HEIGHT OF 3-1/2'.

SIGNALIZED INTERSECTION WITH RIGHT ON RED PERMITTED	
SPEED LIMIT	DISTANCE
40 MPH	325 FEET
35 MPH	250 FEET
30 MPH	200 FEET
25 MPH	150 FEET

MINOR STREETS, PRIVATE ROADS, AND COMMERCIAL DRIVEWAYS	
SPEED LIMIT	DISTANCE
40 MPH	410 FEET
35 MPH	360 FEET
30 MPH	300 FEET
25 MPH	250 FEET

RESIDENTIAL DRIVEWAYS	
SPEED LIMIT	DISTANCE
40 MPH	325 FEET
35 MPH	250 FEET
30 MPH	200 FEET
25 MPH	150 FEET



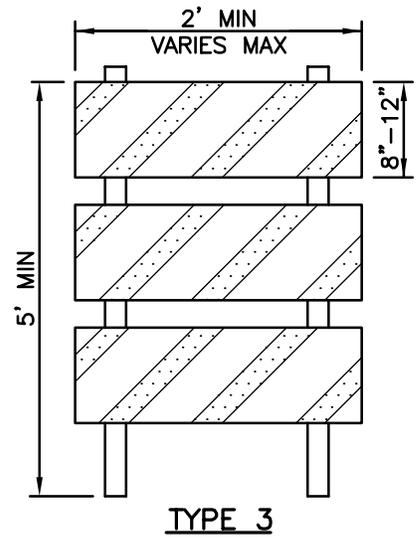
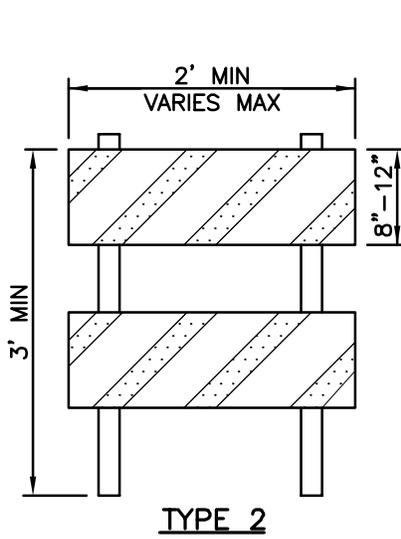
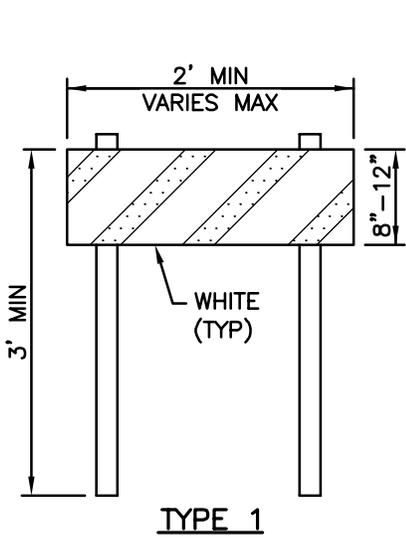
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

**SIGHT DISTANCE -
SETBACK LINES**

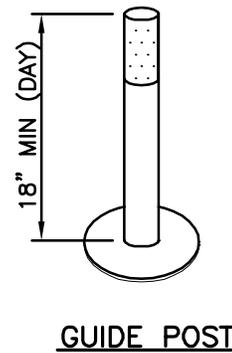
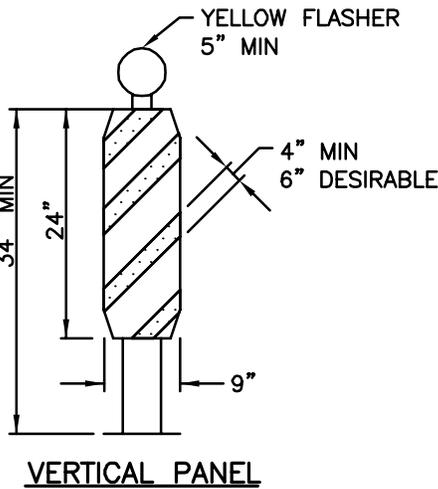
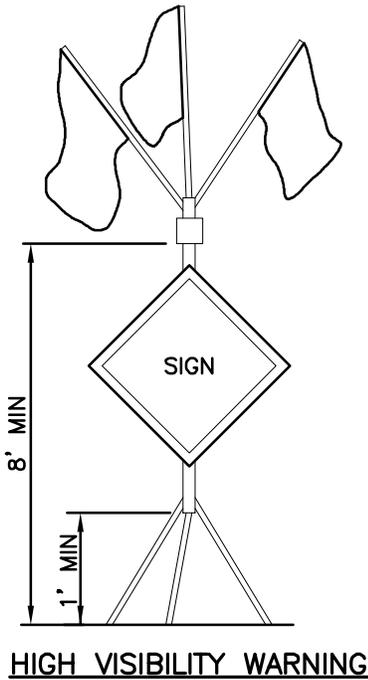
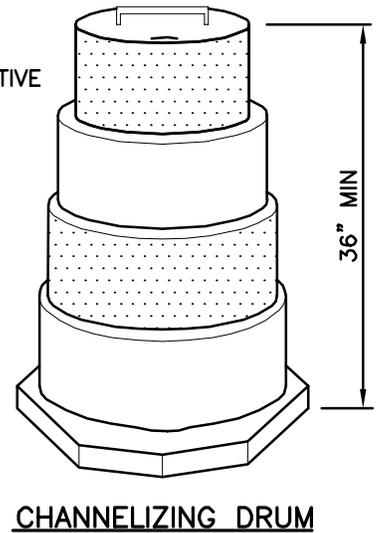
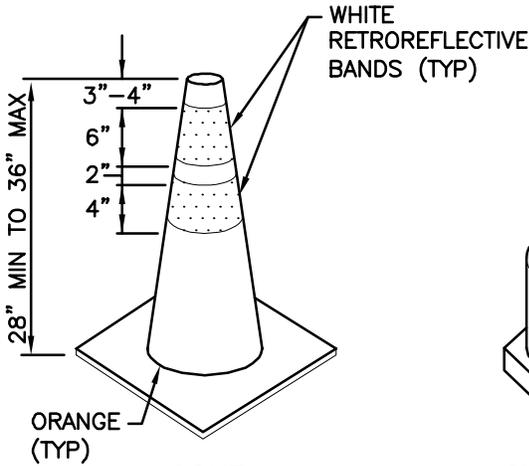
APPROVED BY: **R. ENGLISH**

REVISION DATE
JANUARY 2018

STANDARD
DETAIL
TR-590

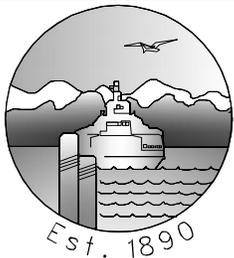


BARRICADES



NOTE:

1. ALL TRAFFIC CONTROL DEVICES SHOULD COMPLY W/ MANUAL UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).



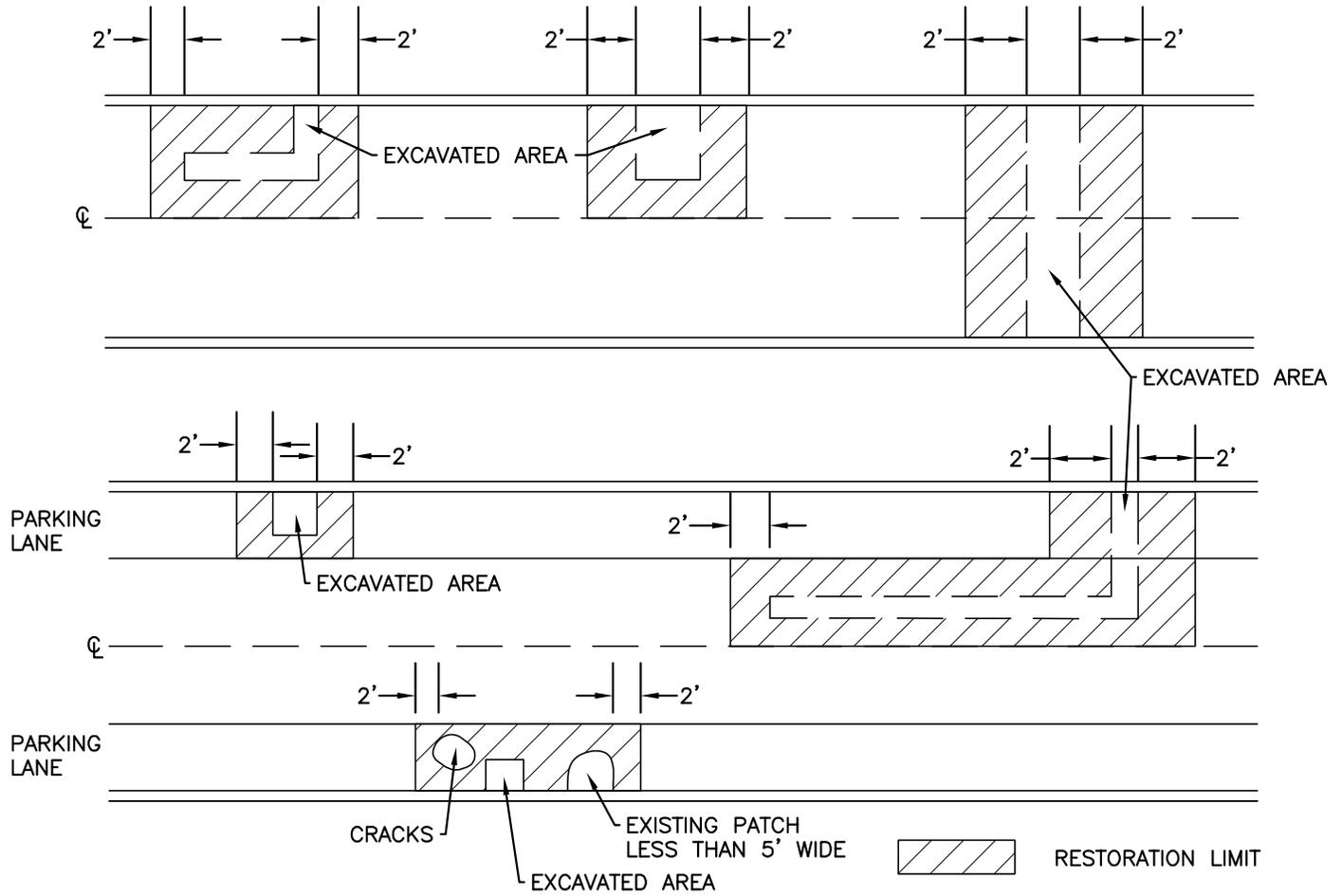
CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

TRAFFIC CONTROL DEVICES

APPROVED BY: **R. ENGLISH**

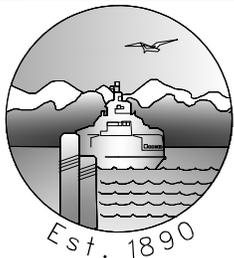
REVISION DATE
JANUARY 2018

STANDARD
DETAIL
TR-591



NOTES:

1. REPAIR AREA MAY BE MODIFIED WITH WRITTEN APPROVAL BY CITY DEPENDING ON CONDITIONS OR SCHEDULED CONSTRUCTION MAINTENANCE
2. ADJUST ALL UTILITY CASTINGS TO FINISHED GRADE AND RESTORE ALL EXISTING SURFACE IMPROVEMENTS INCLUDING STRIPING, SIGNAGE, AND OTHER MARKINGS OR DELINEATORS.



CITY OF EDMONDS
PUBLIC WORKS
DEPARTMENT

ASPHALT REPAIR

APPROVED BY: R. ENGLISH

REVISION DATE
SOMETHING 2019

STANDARD
DETAIL
TR-592